

COMMUNITY DEVELOPMENT
DEPARTMENT
PLANNING DIVISION
301 W. CHESTNUT

PHONE: (479) 621-1186

ROGERS PLANNING COMMISSION AGENDA

Date:	April 16, 2024
Location:	Council Chambers, 301 W. Chestnut Street
Regular Session:	5:30 PM

Online Viewing:

Planning Commission	https://us02web.zoom.us/j/89042525515
Board of Adjustment	N/A
Zoning Review Committee	https://us02web.zoom.us/j/88131527280

DISCLAIMER: The City of Rogers makes no claims, promises, or guarantees regarding the participants ability to attend any public meeting virtually. Technology resources, virtual meeting platforms, and the Internet may occasionally be interrupted or made unavailable by causes beyond the City's reasonable control. The City cannot guarantee that participants will have the opportunity to participate virtually at all times. Public Forums, Public Hearings, and scheduled items of business will not be tabled or postponed due to technological issues. If you are representing a published item of business or wish to speak at a public hearing, in person attendance is required.

REGULAR SESSION

CALL TO ORDER

ROLL CALL

ACTION ON MINUTES

• April 2, 2024

PUBLIC FORUM

REPORTS

- Staff
- Commissioners

DECLARATION OF ABSTENTION

CONSENT AGENDA

1. LSDP: Vin Hotel

A proposal to construct a 53,137 sq. ft. hotel at 5501 W. Northgate Road in the U-COM & U-NBT (Uptown Commercial Mixed Use & Uptown Neighborhood Transition) zoning districts.

- STAFF: Nicholas Little

- REPRESENTED BY: James Dibble

PUBLIC HEARINGS

1. Inform Rogers & Unified Development Code

Regarding adoption of *Inform Rogers*, a new comprehensive plan guiding land use and development for the City of Rogers, and adoption of the Rogers Unified Development Code regulating land use and development for the City of Rogers.

- STAFF: Joe Rexwinkle

2. CUP: William E. Sullivan

A request by William E. Sullivan for a Conditional Use Permit to allow the use "Vehicle/Equipment Sales & Rentals" at 2255 W. Hudson Road in the C-2 (Highway Commercial) zoning district.

- STAFF: Zachery Birdsong

- REPRESENTED BY: William Sullivan

3. RZN: Directline Logistics

A request by Directline Logistics to rezone 2259 N. 2nd Street from the C-2 & I-1 (Highway Commercial and Light Industrial) zoning districts to the I-1 (Light Industrial) zoning district.

- STAFF: Zachery Birdsong

- REPRESENTED BY: Steven Barrett

CALL FOR RECESS

- Zoning Review Committee <u>HELD IN COMMUNITY ROOM</u>
 - Any items referred by Commission
- <u>Board of Adjustment</u> held in Council Chambers

CALL TO ORDER

Planning Commission will reconvene in the Council Chambers.

OLD BUSINESS

1. VAR: The Pointe at Rogers

A request by The Pointe at Rogers to allow Rogers Planning Staff to extend the approval of the LSDP at ± 67.17 acres Near the intersection of S. Champions Drive and W. Pleasant Grove Road in the C-3 (neighborhood commercial) zoning district. (Tabled at 4/2/24 meeting)

- STAFF: Nicholas Little

- REPRESENTED BY: Bill Watkins

NEW BUSINESS

1. All recommended items from committees.

2. LSDP: NWA Drop Yard Gravel Vehicle Storage Waiver

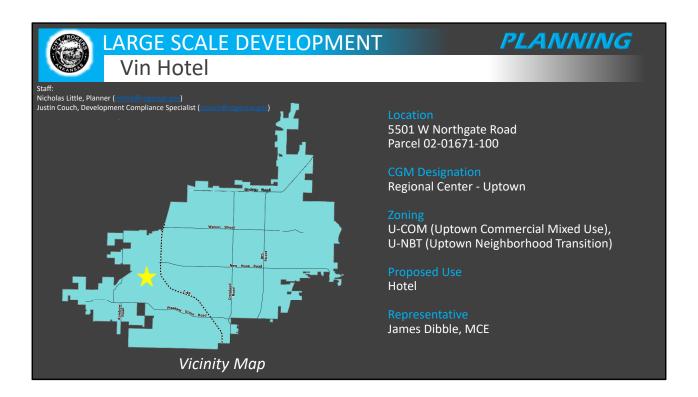
A waiver request by NWA Drop Yard for paved parking at 2609 W. Hudson Road in the I-1, CU (Light Industrial w/ Condominium Unit) zoning district.

- STAFF: Nicholas Little

- REPRESENTED BY: Chad Caletka

COMMISSIONERS' FINAL COMMENTS

ADJOURN



Location:

West of the intersection of W Northgate Road and S Champions Road

Planning Review:

COMPREHENSIVE GROWTH MAP:

Growth Designation:

Regional Center

Growth Designation Character:

Mixed-use urban cores that function as regional activity hubs for commerce, arts and entertainment, living, recreation, and retail. Intensity supported by well-connected, high-capacity, pedestrian-oriented street network. Goals include high-quality design and efficient use of land. (CGM PAGE 1)

Allowed Zoning Districts:

U-COM, U-NBT

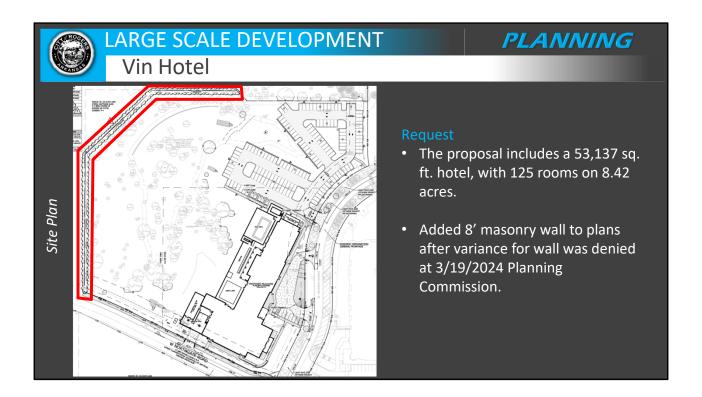


ZONING:

UCOM(Uptown Commercial Mixed Use):

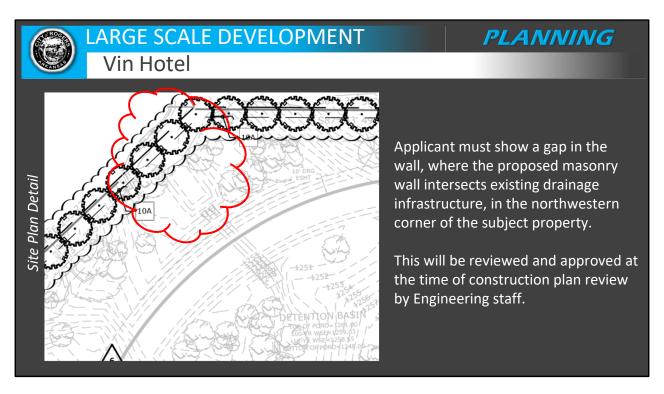
Purpose and intent. The purpose of the Uptown Commercial Mixed Use zone is to be compatible with U-COR through flexibility of uses, continuing to allow retail and commercial as primary uses, but also including high-density residential as a core use to be located within Uptown Rogers. The focus is to strike a balance between conventional forms of commercial development and high-intensity urban mixed-use development. Development Standards within this URDC zone specifically address the unique aspects of development in this area by respecting the need for medium-intensity development between U-COR and existing residential areas.

Sec. 14-732(4.3).



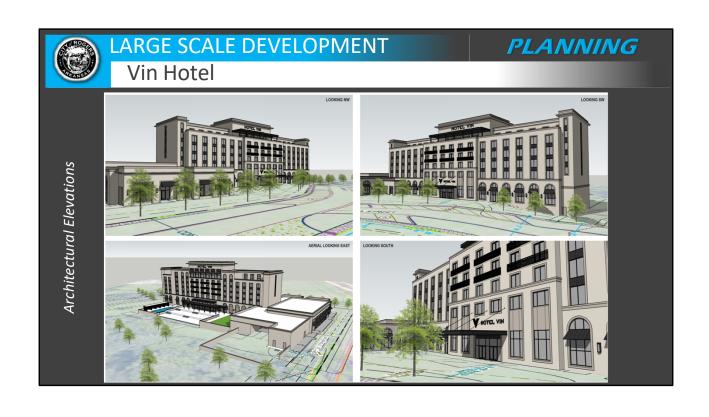
• The proposal includes a 53,137 sq. ft. hotel, with 125 rooms on 8.42 acres.

Added 8' masonry wall to plans after variance for wall was denied at 3/19/2024 Planning Commission.



Applicant must show a gap in the wall, where the proposed masonry wall intersects existing drainage infrastructure, in the northwestern corner of the subject property.

This will be reviewed and approved at the time of construction plan review by Engineering staff.





<u>Summary</u> <u>LSDP Staff Recommendation</u>

APPROVE

CITY GENERAL NOTES

- IT IS THE RESPONSIBILITY OF THE ENGINEER OF RECORD TO INCLUDE ALL ITEMS REQUIRED BY ORDINANCE. IF APPROVED PLANS CONFLICT WITH ORDINANCE REQUIREMENTS, THE ORDINANCE TAKES PRECEDENCE UNLESS A WAIVER OR
- STOCKPILES WITH EXCESS CONSTRUCTION SITE MATERIALS, INCLUDING BUT NOT LIMITED TO EXCESS TOP SOIL STOCKPILES, MUST BE REMOVED FROM SITE PRIOR TO PROJECT COMPLETION. GRADING BEYOND THE APPROVED LIMITS IS
- CHANNELS, DETENTION PONDS, RETENTION PONDS, SEDIMENT BASINS, ETC. REQUIRE SOD TO THE 100 YEAR WATER SURFACE ELEVATION WITHIN 2 WEEKS (14 DAYS) AFTER THE POND IS BUILT, AND MUST BE INSTALLED AT THE BEGINNING OF THE PROJECT WHERE FEASIBLE
- WHERE PUBLIC STREET END FOR FUTURE STUBOUTS, INSTALL MUTCD CLASS 3 BARRICADE WITH "FUTURE STREET EXTENSION" SIGNAGE CENTERED AT THE END

MEASURED FROM EDGE OF ASPHALT TO EDGE OF ASPHALT.

WHERE PUBLIC SIDEWALKS, TRAILS, OR SIDEPATHS END, INCLUDE "SIDEWALK PUBLIC STREET AND ALLEYWAYS SHALL HAVE 20-FT OF DRIVABLE SURFACE FOR TWO WAY TRAFFIC. THIS DOES NOT INCLUDE THE CURB AND GUTTER AND IS

LEGAL DESCRIPTION

LOT 3 OF THE LOT SPLIT OF LOTS 1 & 2 PER PROPERTY LINE ADJUSTMENT 2016-86 CREATING LOT 1-4 AS FILED FOR RECORD 3/16/2023

LAND USE

HOTEL SPACE

GENERAL NOTES

INFORMATION CONCERNING UNDERGROUND UTILITIES WAS OBTAINED FROM AVAILABLE RECORDS AND EXISTING FIELD CONDITIONS WHEN POSSIBLE. THE CONTRACTOR MUST DETERMINE THE EXACT LOCATION AND ELEVATION OF ALL EXISTING UTILITIES BY HAND DIGGING TEST PITS AT ALL UTILITY CROSSINGS IN ADVANCE OF TRENCHING WITH ANY MECHANICAL DEVICE. ANY CLEARANCES LESS THAN SPECIFIED ON THESE PLANS OR 18" (WHICHEVER IS LESS), CONTRACTOR SHALL CONTACT McCLELLAND CONSULTING ENGINEERS, INC. @ (479) 443-2377 PRIOR TO

CONTRACTOR SHALL BE RESPONSIBLE FOR RELOCATION OR REMOVAL OF EXISTING UNDERGROUND UTILITIES SHOWN OR NOT SHOWN AT NO ADDITIONAL COST TO THE

CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND USE OF ANY AND ALL REQUIRED OR NECESSARY SHORING, SHEETING, BRACING, AND SPECIAL EXCAVATION MEASURES REQUIRED ON THIS PROJECT, THESE MEASURES WILL MEET OR EXCEED OSHA, FEDERAL, STATE, AND LOCAL REGULATIONS DURING CONSTRUCTION McCLELLAND CONSULTING ENGINEERS, INC. AND THE DEVELOPER/OWNER ACCEPTS NO RESPONSIBILITY FOR THE CONTRACTORS FAILURE TO COMPLY WITH THESE

ALL UTILITIES, INCLUDING STORM SEWER, SHOWN WITHIN PUBLIC EASEMENTS OR RIGHT OF WAYS SHALL BE CONSTRUCTED TO THE GOVERNING AGENCY'S SPECIFICATIONS. ALL OTHER UTILITIES SHALL BE CONSTRUCTED TO THE CLIENT'S OR THE GOVERNING AGENCY'S SPECIFICATIONS, WHICHEVER IS MORE STRINGENT. IF THERE IS A QUESTION AS TO WHICH SPECIFICATIONS SHOULD APPLY, THE CONTRACTOR SHALL CONTACT THE ENGINEER OF RECORD.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FOLLOW ALL RECOMMENDATIONS SET FORTH IN THE GEOTECHNICAL STUDY. WHERE A CONFLICT EXISTS BETWEEN THE HIGHWAY DEPARTMENT / CITY STANDARDS, OR THE GEOTECH STUDY, THE MOST STRINGENT SHALL APPLY

FURTHERMORE IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAINTAIN THE EROSION CONTROL MEASURES AS SHOWN ON THESE PLANS. CONTRACTOR SHALL KEEP THESE MAINTAINED THROUGHOUT ALL PHASES OF CONSTRUCTION. CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR IMPLEMENTING THE SWPPP BEFORE, DURING, AND

ALL PHASES OF CONSTRUCTION SHALL MEET OR EXCEED THE OWNER / DEVELOPER

SEE ARCHITECTURAL PLANS FOR THE EXACT DIMENSIONS AND LOCATIONS OF ANY RAMPS, VESTIBULE, PORCHES, SLOPED PAVING, BUILDING UTILITY ENTRANCE LOCATIONS, TRUCK DOCKS, OR WELLS AND PRECISE BUILDING DIMENSIONS.

CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH, AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION PROCEDURES. DISPOSAL SHALL BE IN ACCORDANCE WITH ALL LOCAL, STATE, AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.

THE BOUNDARY / TOPOGRAPHIC SURVEY HAS BEEN PROVIDED BY THE FOLLOWING

McCLELLAND CONSULTING ENGINEERS, INC. 1580 E. STEARNS STREET

OFFICE: (479) 443-2377

FAX: (479) 443-9241 McCLELLAND CONSULTING ENGINEERS, INC. AND ITS ASSOCIATES WILL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OF THE SURVEY OR FOR DESIGN ERRORS OR OMISSIONS RESULTING FROM SURVEY INACCURACIES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL EXISTING FEATURES AS SHOWN ON THE SURVEY AND NOTIFY THE SURVEYOR AND THE ENGINEER OF RECORD OF ANY INACCURACIES FOUND ON THE SURVEY PRIOR TO BEGINNING CONSTRUCTION.

THE GENERAL CONTRACTOR SHALL BE HELD SOLELY RESPONSIBLE FOR AND SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.

ALL CONSTRUCTION IN THE STATE RIGHT-OF-WAY SHALL BE COORDINATED WITH THE

THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING ALL PHASES OF CONSTRUCTION OF THIS SITE. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND SHALL NOT BE LIMITED TO CONSTRUCTION WORKING HOURS.

THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR DESIGN FUNCTION AND USE INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER THE ENGINEER NOR HIS PERSONNEL CAN OR DO WARRANT THESE DESIGNS OR PLANS AS CONSTRUCTED EXCEPT IN THE SPECIFIC CASES WHERE THE ENGINEER INSPECTS AND CONTROLS THE PHYSICAL CONSTRUCTION AT THE SITE.

WETLAND NOTE

THERE ARE POTENTIAL WETLAND AREAS THAT EXIST WITHIN THE PROJECT BOUNDARIES OF THIS SITE AS NOTED PER THE US FISH & WILDLIFE WETLAND MAPPER.

FLOOD CERTIFICATION

THIS ENTIRE PROPERTY IS LOCATED WITHIN UNSHADED FLOOD ZONE "X", OR AREAS OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM FLOOD INSURANCE RATE MAP FOR BENTON COUNTY,

FIRM PANEL: #05007C0265K EFFECTIVE DATE: 6/5/2012

CONSTRUCTION TYPE

BUILDING CODE 2021 AR BUILDING CODE

2021 AR FIRE PREVENTION CODE

2021 AR MECHANICAL CODE 2018 AR PLUMBING CODE

2020 NATIONAL ELECTRIC CODE

HOTEL VIN FIELDS AT PINNACLE LOT 3

5501 W. MADISON AVENUE, ROGERS, AR

LARGE SCALE DEVELOPMENT

CITYVIEW PROJECT #PL202300974 (URDC ZONE)

SITE DATA (LOT 3)				
OVERALL SITE AREA	366,752 S.F.	8.42 AC		
TOTAL DEVELOPMENT (DISTURBED) AREA	222,602 S.F.	5.11 AC 6		
ZONING	MIXED USE) & U	GERS DEVELOPMENT TOWN COMMERCIAL J-NBT (UPTOWN D TRANSITION)		
FRONT BUILD-TO ZONE	10' MIN 45' MAX. (GENERAL FRONTAGE)		
SIDE BUILD-TO ZONE	0' MIN.; NO MAX. (GENERAL FRONTAGE)			
REAR BUILD-TO ZONE	0' MIN.; NO MAX. (GENERAL FRONTAGE)			
BUILDING COVERAGE AREA	53,137 SF (14	.49% OF LOT)		
TOTAL IMPERVIOUS AREA	124,897 SF (34	4.05% OF LOT)		
TOTAL PERVIOUS AREA	241,855 SF (6	5.95% OF LOT)		
TOTALY BUILDING HEIGHT		-0 6 \		
OPEN SPACE REQUIREMENTS	REQUIRED	PROVIDED		
GENERAL REQUIREMENT (5% OF TOTAL LOT)	18,338 SF	19,150 SF (103.62% TOTAL OPEN SPACE)		
NON-RESIDENTIAL (4 SF / 1K SF BUILDING COVERAGE AREA)	143 SF	1,500 SF (8.12% TOTAL OPEN SPACE)		
TOTAL OPEN SPACE	18,481 SF	20,650 SF (111.74% TOTAL OPEN SPACE)		

OTTE D 4T4 (LOT 0)

CONTACTS

CITY OF ROGERS 301 W CHESTNUT STREET ROGERS, AR 72756 JOHN McCURDY / DIRECTOR PHONE: (479) 621-1186

EMAIL: jmccurdy@rogersar.gov

EMAIL: ljobe@rogersar.gov

301 W. CHESTNUT STREET ROGERS, AR 72756 PHONE: (479) 621-1186

601 S. 2nd STREET

PHONE: (479) 936-5425 EMAIL: earlrousche@rwu.org

201 N. 1st STRFFT ROGERS, AR 72756 THOMAS JENKINS PHONE: (479) 621-1179

CARROLL ELECTRIC CO-OP 707 SE WALTON BOULEVARD, BENTONVILLE, AR 72712 DEREK THURMAN EMAIL: dthurman@carrollecc.com PHONE: (479) 273-2421 x 2646

EMAIL: btuttle@carrollecc.com

700 W. WALNUT, ROGERS, AR 72756 BRIAN WILLIAMS PHONE: (479) 249-5194 EMAIL: bw9156@att.com

CHAD HODGE PHONE: (479) 871-0339 EMAIL: chad.hodge@cox.com PHONE: (479) 387-8305 EMAIL: tim.busey@cox.com EMAIL: dion.jones@cox.com PHONE: (479) 721-6265

RITTER COMMUNICATIONS sam.valle@rittercommunications.com

CITY OF ROGERS FIRE DEPARTMENT

1301 FEDERAL WAY, LOWELL, AR 72745 ADAM COMER / UTILITY PLANNER PHONE: (479) 877-0006

TELEPHONE / INTERNET

SAM VALLE PHONE: (479) 212-0141

(INFRASTRUCTURE PACKAGE) IATFIELD WHALEN LAND, LLC 3070 S. CHAMPIONS DRIVE, STE. 101, ROGERS, AR 72758

PHONE: (479) 619-6334 EMAIL: chad@hatfieldpropertiesllc.com (LOT 2 PACKAGE) GREAT LAKES CAPITA EMAIL: adam.comer@blackhillscorp.com

112 W. JEFFERSON BOULEVARD, STE. 200 SOUTH BEND, IN 46601 KRISTEN SAWDON PHONE: (574) 213-5615 EMAIL: ksawdon@greatlakescapital.com **CIVIL ENGINEERING CONSULTANT**

CHAD HATFIELD

1580 E. STEARNS STREET. FAYETTEVILLE, AT 72703 CHRIS BAKUNAS, PLA / PROJECT MANAGER

EMAIL: cbakunas@mce.us.com

PHONE: (479) 443-2377

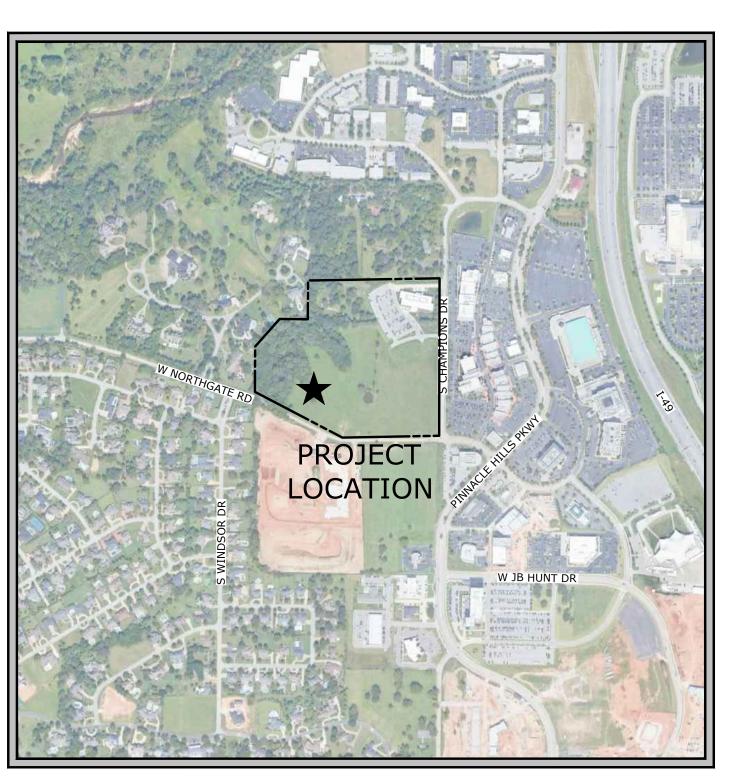
NOTE: A STATE CONTRACTOR'S LICENSE IS REQUIRED FOR PROJECTS EXCEEDING IOTE: EASEMENT PLAT IS REQUIRED FOR CERTIFICATE OF OCCUPANCY NOTE: A SIGN PERMIT IS REQUIRED SEPARATE FROM LSD REVIEW / APPROVAL NOTE: ALL HVAC MUST BE SCREENED FROM PUBLIC VIEW NOTE: OWNER / DEVELOPER MUST PROVIDE A 3-YEAR GUARANTEE, STARTING ONCE INSTALLATION IS COMPLETE, FOR MAINTAINING & REPLACING LANDSCAPING BEFORE ISSUANCE OF A CERTIFICATE OF OCCUPANCY

NOTE: PLANTINGS THAT EXCEED THE MINIMUM REQUIREMENTS MAY NOT BE REDUCED

: ALL CIVIL PLANS USED ON SITE MUST INCLUDE MOST CURRENT STAMP BY

ONCE APPROVED BY PLANNING COMMISSION

REVISIONS DATE DESCRIPTION 1/11/2024 | ADDRESSING CITY LSD & RWU COMMENTS 2/28/2024 ADDRESSING CITY LSD & RWU COMMENTS 3/13/2024 | ADDRESSING RWU COMMENTS 3/20/2024 | ADDENDUM #1 4/5/2024 | ADDRESSING PLANNING COMMISSION COMMENTS



VICINITY MAP

April 5, 2024 MCE PROJECT NUMBER 22-2174

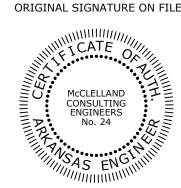


http://www.mce.us.com

1580 E STEARNS ST FAYETTEVILLE, ARKANSAS 72703 (479) 443-2377

LITTLE ROCK, AR ● FAYETTEVILLE, AR ● FORT SMITH, AR

PRELIMINARY NOT FOR



CONSTRUCTION

C7.07 CITY OF ROGERS DETAIL SHEET

RWU STANDARD DETAILS SUPPLEMENTAL SHEETS

BUILDING ELEVATIONS DUMPSTER ENCLOSURE WALL SITE LIGHTING PLAN

INDEX TO DRAWINGS

BOUNDARY & TOPOGRAPHIC SURVEY

C0.01 CIVIL GENERAL NOTES

C2.01 SITE PLAN - NORTH

C2.02 SITE PLAN - NORTHWEST

C2.04 SITE PLAN - SOUTHWEST

C3.01 GRADING PLAN - NORTH

C3.02 GRADING PLAN - SOUTH

C3.05 GRADING PLAN - INSETS II

C3.06 DRAINAGE PLAN - NORTH

C3.07 DRAINAGE PLAN - SOUTH

C3.08 DRAINAGE PLAN - WEST

C3.09 DRAINAGE PROFILES I

C3.10 DRAINAGE PROFILES II

C3.11 DRAINAGE PROFILES III

C3.12 DRAINAGE PROFILES IV

C3.13 DRAINAGE PROFILES V

C4.01 UTILITY PLAN - NORTH

C4.02 UTILITY PLAN - SOUTH

C4.04 UTILITY PLAN & PROFILES

C5.00 PHASE I EROSION CONTROL PLAN

C5.01 PHASE II EROSION CONTROL PLAN

C6.00 TREE MITIGATION PLAN

C6.02 LANDSCAPE NOTES & DETAILS

C4.03 UTILITY PLAN - WEST

C6.01 LANDSCAPE PLAN

C7.00 DETAIL SHEET I

C7.02 DETAIL SHEET III

C7.03 DETAIL SHEET IV

C7.04 DETAIL SHEET V

C7.05 DETAIL SHEET VI

C7.06 DETAIL SHEET VII

DETAIL SHEET II

OVERALL UTILITY PLAN

C3.03 GRADING PLAN - WEST

C1.00 OVERALL DEMOLITION PLAN

C1.01 DEMOLITION PLAN - NORTH

C1.02 DEMOLITION PLAN - NORTHWEST

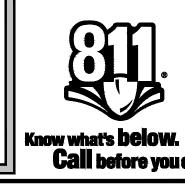
C1.04 DEMOLITION PLAN - SOUTHWEST

DEMOLITION PLAN - SOUTH

C3.00 OVERALL GRADING & DRAINAGE PLAN

GRADING PLAN - INSETS I

C0.00 COVER



REV / BID /

CO / ADD

REV-6

REV-2

REV-6

REV-3

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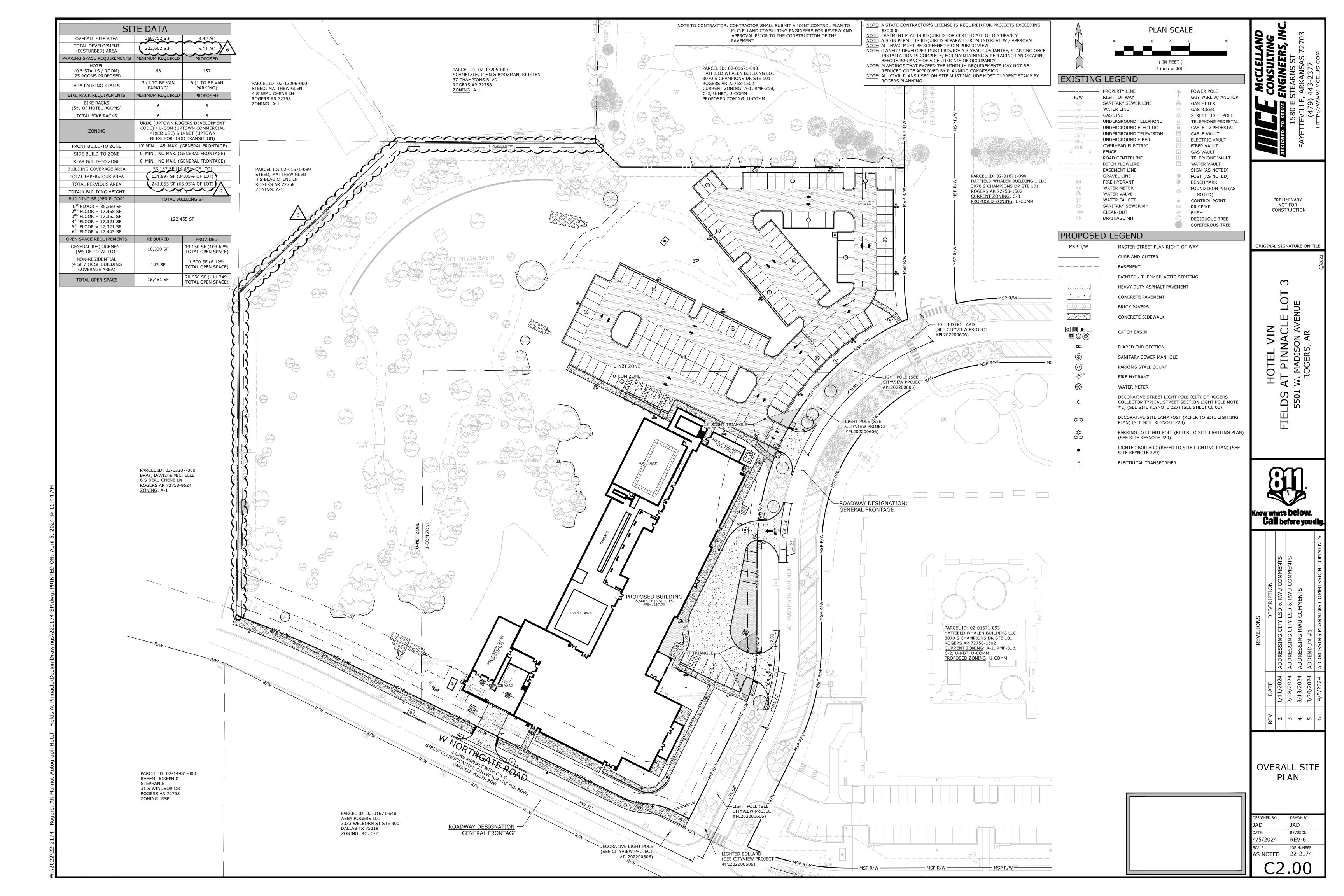
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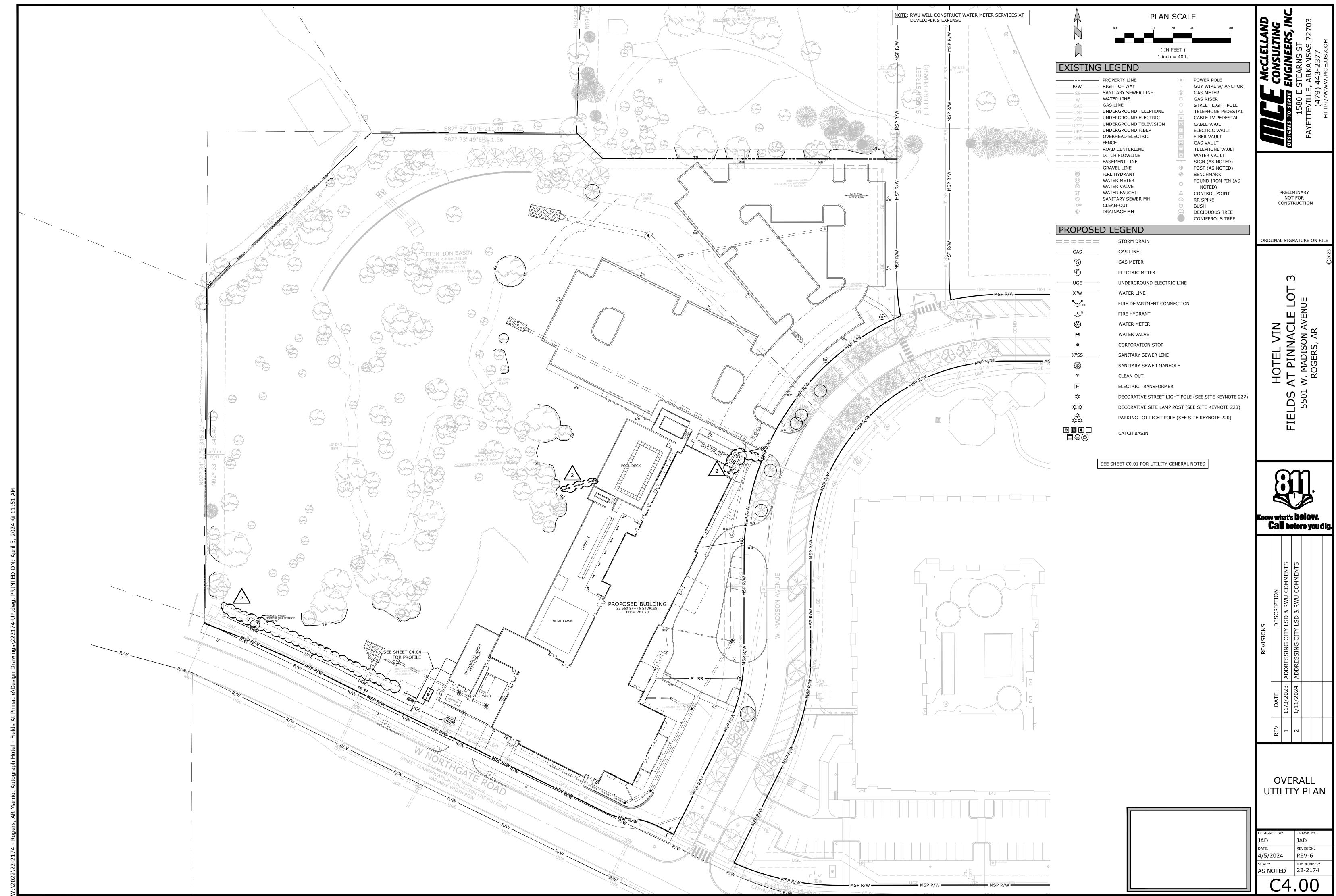
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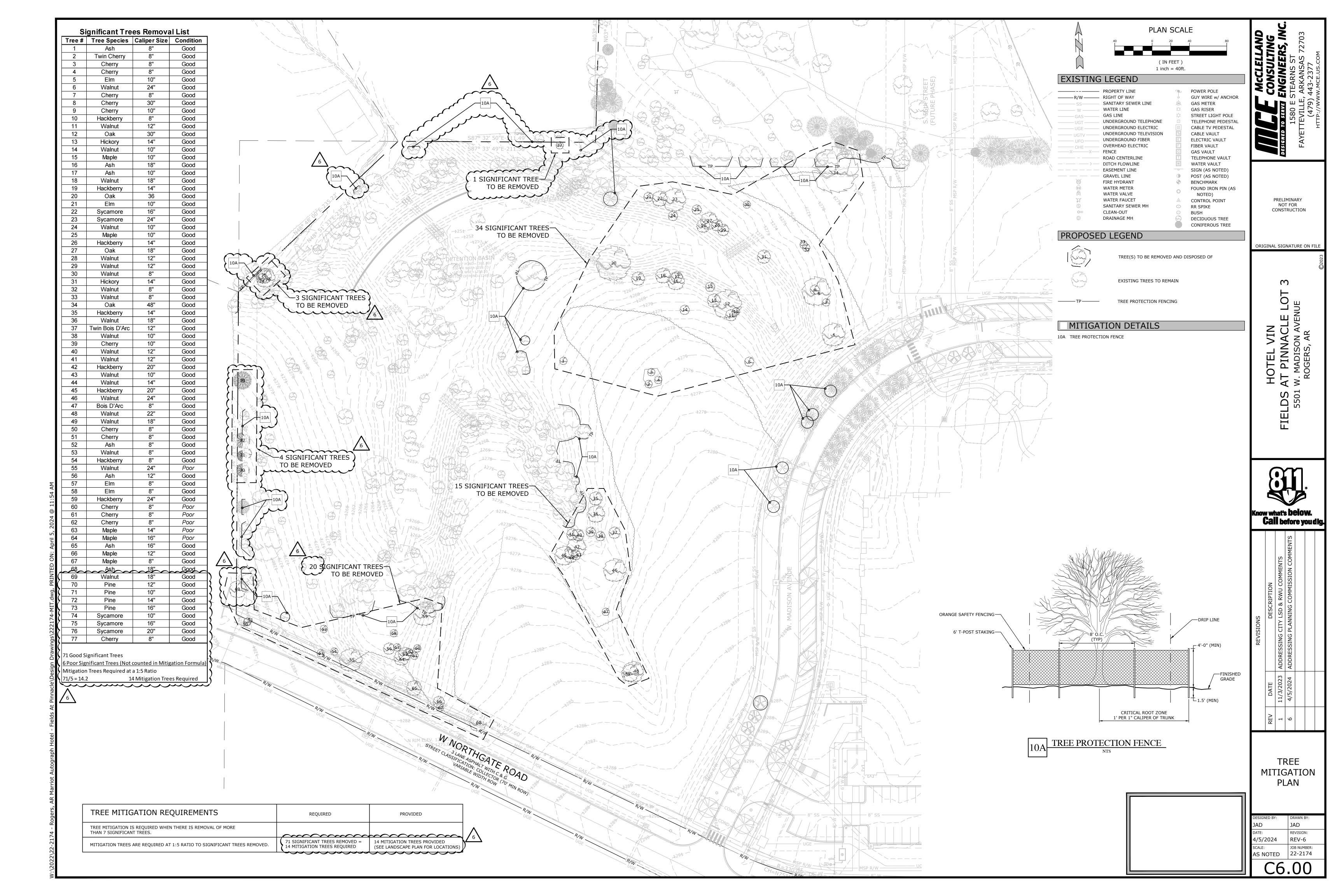
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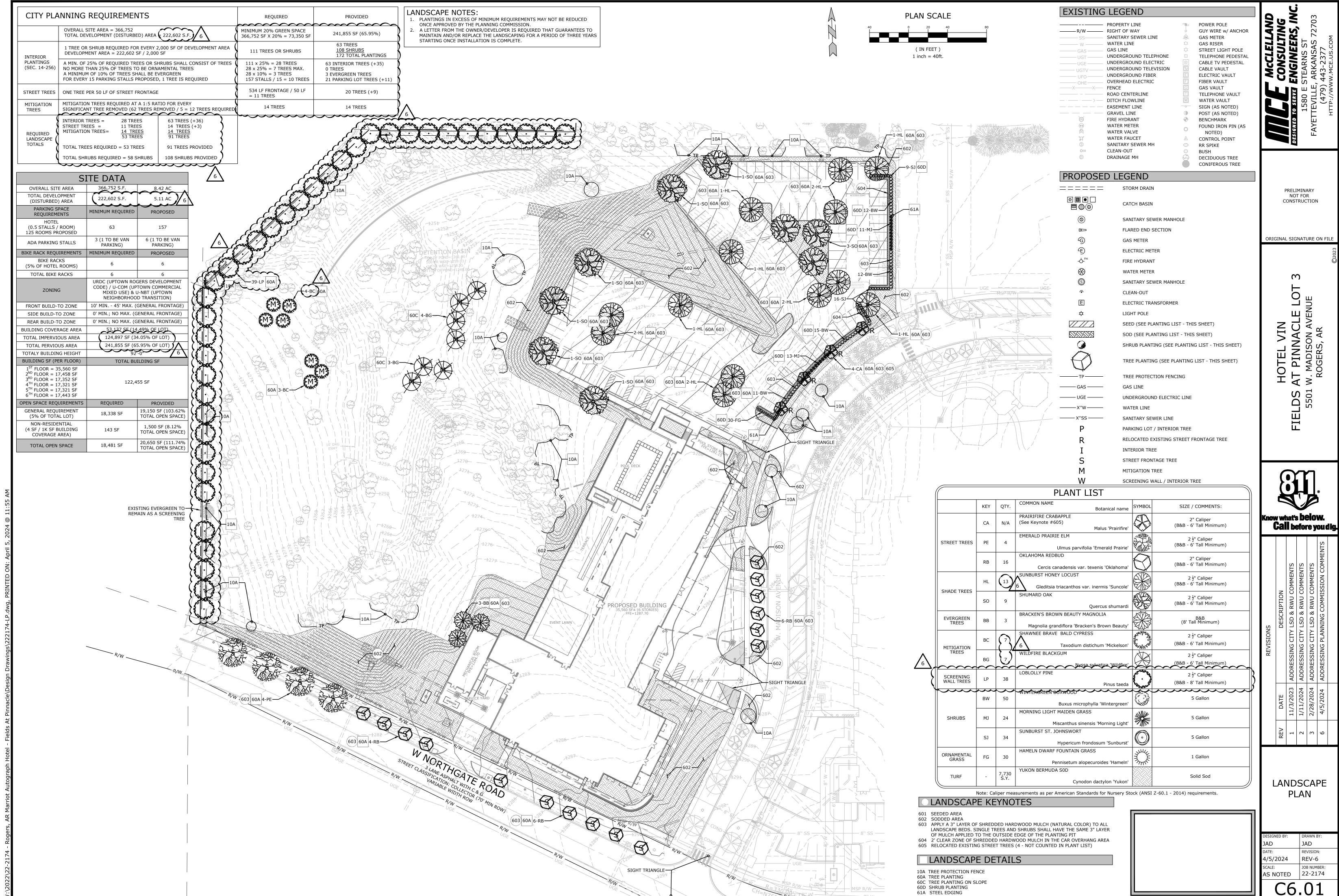
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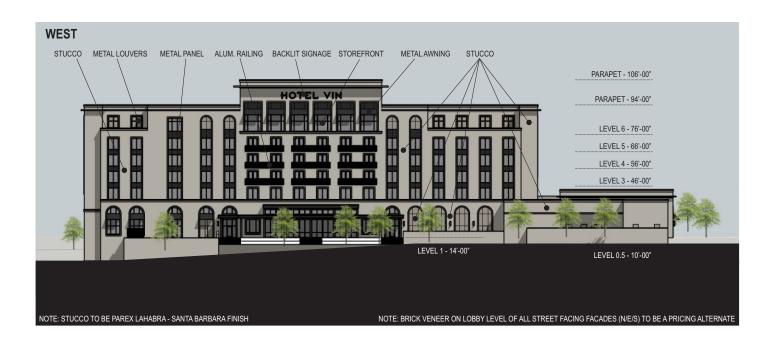
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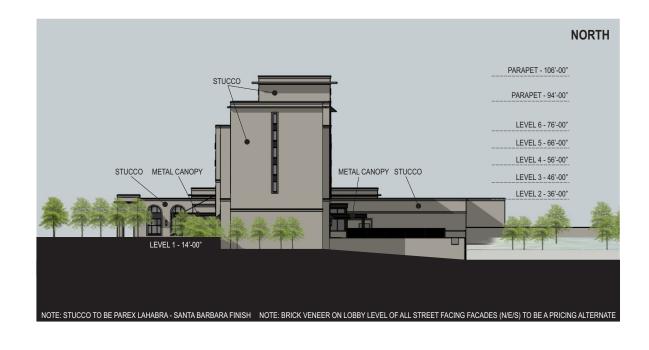


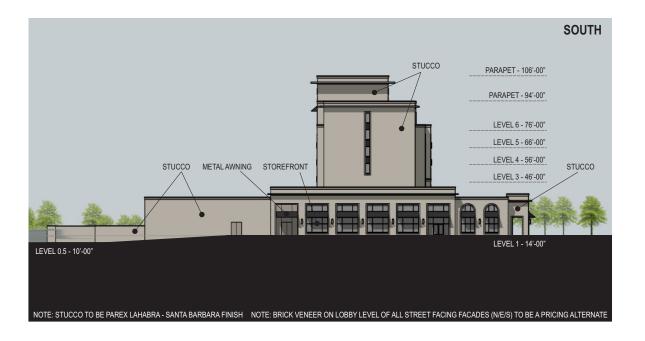


Exterior Elevations













Exterior Views No Scale











Exterior Views No Scale







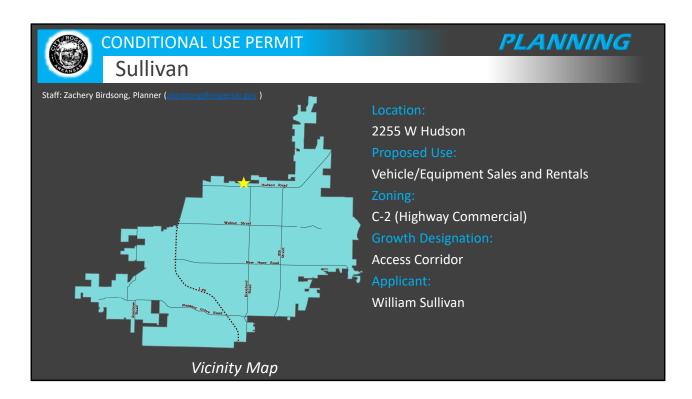












Summary:

This conditional use request is located in northern Rogers at 2255 W Hudson Road.



Geolocation:

It is located to at the northwest corner of West Hudson and North 22nd Street, north of Rogers Regional Sports Park.



Proposal

The applicant is requesting to have a conditional use permit for a Vehicle/Equipment Sales and Rentals.

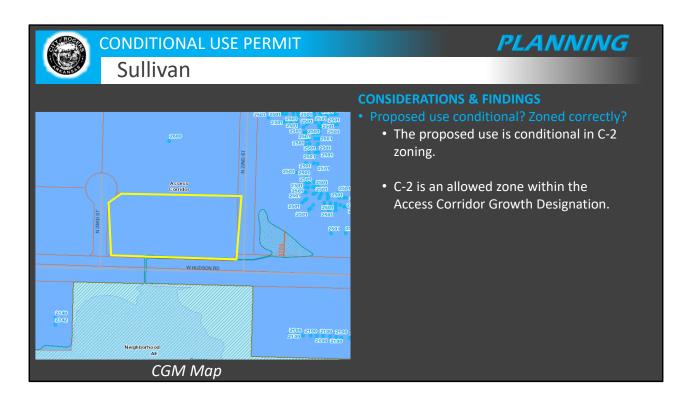
ZONING:

C-2 (Highway Commercial):

"This district is intended for commercial uses, which depend upon high visibility, generate high traffic volumes or cater to the traveling public. These characteristics dictate that this district be located along or at the intersections of arterial classification streets or along frontage roads adjacent to the interstate or other limited-access streets." Sec. 14-709(a).

Use Definition:

"Vehicle/Equipment Sales and Rentals is a secondary use classification encompassing the sale or rental of automobiles, motorcycles, trucks, tractors, construction equipment, agricultural equipment, mobile homes, and similar equipment." Sec. 14-695(b)(2)(mm)(1)



Is the proposed use listed as conditional in the subject zoning district and is the property zoned correctly?

Vehicle/Equipment Sales and rentals is a conditional use in C-2 zoning. The current zone is also allowed within the Access Corridor Growth Designation.

Growth Designation:

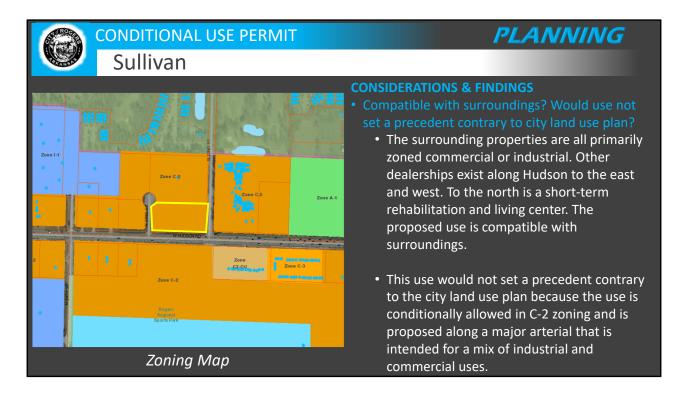
Access Corridor

Growth Designation Purpose:

To improve existing commercial corridors by reducing traffic congestion and pedestrian unfriendliness through redevelopment and integration of residential uses. (CGM Page 1).

Allowed Zoning Districts:

R-MF, R-O, C-2



Is the proposed use compatible with adjacent property? Would proposed use set a precedent contrary to future land use plan?

- The surrounding properties are all primarily zoned commercial or industrial. Other
 dealerships exist along Hudson to the east and west. To the north is a short-term
 rehabilitation and living center. The proposed use is compatible with surroundings.
- The use would not set a precedent contrary to the city land use plan. The use is conditionally allowed in C-2 and is proposed along a major arterial that is intended to contain a mix of industrial and commercial uses. The use is also not a high trip generator.

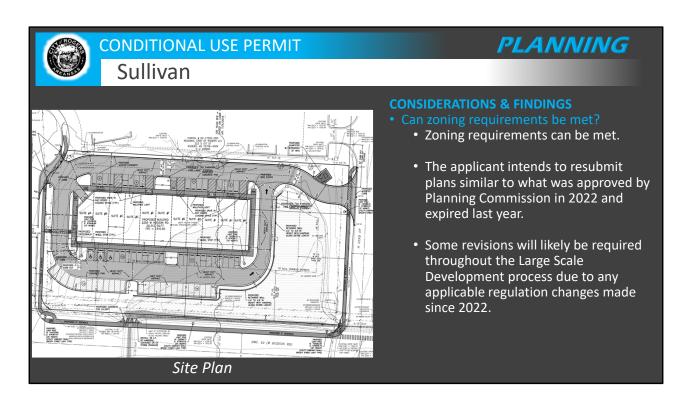
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C-2 (Highway Commercial):

"This district is intended for commercial uses, which depend upon high visibility, generate high traffic volumes or cater to the traveling public. These characteristics dictate that this district be located along or at the intersections of arterial classification streets or along frontage roads adjacent to the interstate or other limited-access streets." Sec. 14-709(a).

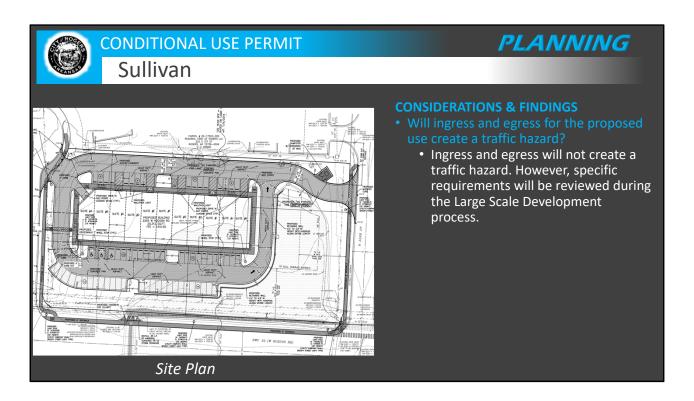
Use Definition:

"Vehicle/Equipment Sales and Rentals is a secondary use classification encompassing the sale or rental of automobiles, motorcycles, trucks, tractors, construction equipment, agricultural equipment, mobile homes, and similar equipment." Sec. 14-695(b)(2)(mm)(1)



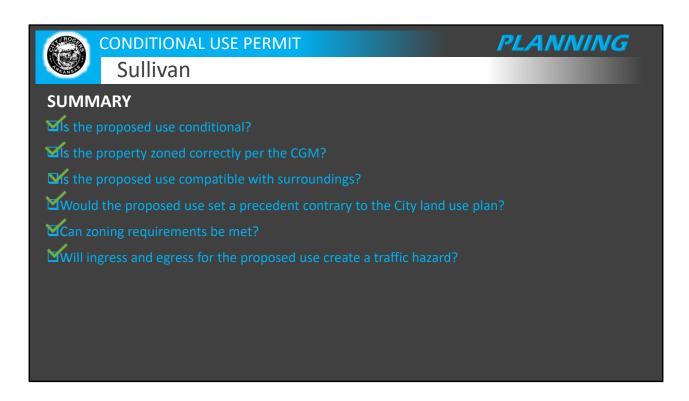
Can all other zoning requirements be met?

- Zoning requirements can be met.
- The applicant intends to resubmit plans similar to what was approved by Planning Commission in 2022 and expired last year.
- Some revisions will likely be required throughout the Large Scale Development process due to any applicable regulation changes made since 2022.



Will ingress and egress for the proposed use create a traffic hazard?

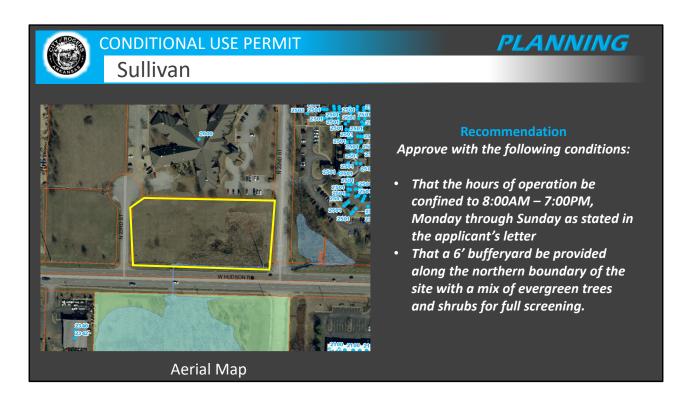
• Ingress and egress here would not create any traffic hazards. Although, specific requirements will be reviewed during the Large Scale Development process.



CONSIDERATIONS & FINDINGS PER SEC. 14-723(a)(3):

In Summary:

All conditional use considerations are met.



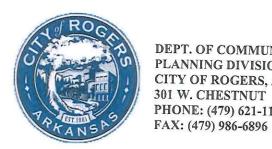
Public Input:

No public input has been received.

RECOMMENDATION:

Approve with the following conditions:

- That the hours of operation be confined to 8:00AM 7:00 PM, Monday through Sunday as stated in the applicant's letter
- That a 6' bufferyard be provided along the northern boundary of the site with a mix of evergreen trees and shrubs for full screening.



DEPT. OF COMMUNITY DEVELOPMENT PLANNING DIVISION CITY OF ROGERS, ARKANSAS 301 W. CHESTNUT PHONE: (479) 621-1186

OFFICE USE C	DNLY
Permit Fee:	(\$100)
Zoning:	
Permit Number:	
CityView Application:	
Date:	

CONDITIONAL USE PERMIT
APPLICANT: William E Sullivan Jr
ADDRESS: 4701 Clydesdale Dr, Flower Mound, TX 75028 SUITE #: N/A
GENERAL LOCATION OF PROPERTY: 2255 W Hudson RD, Rogers, AR 72756
PARCEL NUMBER: 02-17521-000
PHONE #: 972-880-9500 EMAIL: willytundra@gmail.com
PROPERTY OWNER: Louis & Shannon Froud PHONE #: 501-388-0935
PRESENT USE: None ZONING:
PROPOSED CONDITIONAL USE: Motorcycle Dealership
PARKING SPACES AVAILABLE: 60 HOURS OF OPERATION: 8:00am - 7:00pm Mon-Sui
IF APPLYING TO OPEN A DAYCARE:
NUMBER OF CHILDREN: N/A MOST CHILDREN AT ONE TIME: N/A
WUSSU 3/1/24
Applicant Signature Date
 Is the property switching from a residential to non-residential use? Yes. If so, large-scale approval and/or coordination with Risk Reduction for compliance with the appropriate building codes will be required. No
Attachment Checklist:
Letter explaining request
Parcel Number Applicant Certification
✓ Applicant Certification ✓ Site plan as needed
X Letter of Approval from Property Owner
PLANNING STAFF PROVIDES:
DATE FILED: PUBLIC HEARING DATE: CERTIFIED MAIL DATE:
PLANNING COMMISSION ACTION:
COMMENTS, CONDITIONS, LIMITS:

William E Sullivan Jr 4701 Clydesdale Dr. Flower Mound, TX 75028

March 1, 2024

Department of Community Development Planning Division City of Rogers, Arkansas 301 W. Chestnut

Re: Conditional Use Permit

I am in the process of purchasing a property, parcel number 02-17521-000, located at 2255 W. Hudson Rd just east of the Harley dealership to build and operate a BMW Motorrad & Ducati dealership. With the understanding that it is zoned C-2, I am desiring to secure a Special Use Permit to operate a motorcycle dealership and display motorcycles outside as traditional with motorcycle dealership operations.

<u>Additional Information:</u>

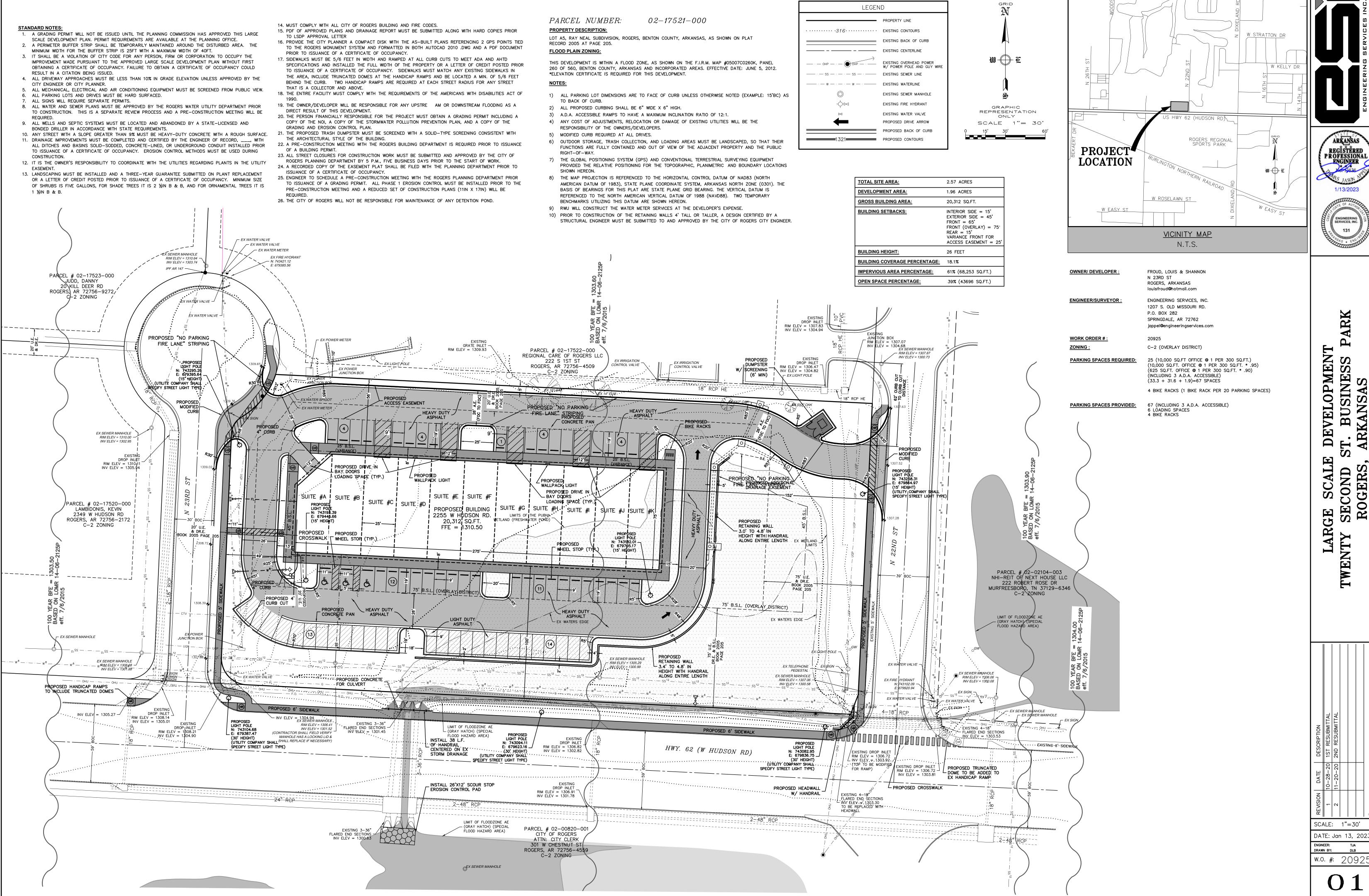
- Type of business to be operated is a motorcycle dealership providing motorcycle sales, service and parts sales.
- The proposed hours of operation are Monday Sunday, 8:00am 7:00pm.
- Additional traffic may yield 10-15 automobiles and 5 10 motorcycles per day.
- No additional parking is required outside the proposed site plan.
- Signage will consist of a monument and building sign as allowed by signage code and requested by motorcycle manufacturers.
- Flammable or hazardous materials will be comprised of typical motorcycle dealership products such as engine oil, small fuel containers, and other related products.
- Any noise generated will meet city sound ordinances. Pole mounted parking lights with photometric plan will be installed.
- A rear fence will be installed to screen any surrounding businesses or residential areas.

Please let me know if you need any additional information,

Kindest regards,

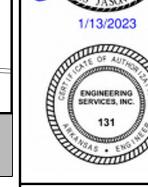
Willy Sullivan 972-880-9500

willytundra@gmail.com









SINES GE SCALE SECOND ROGERS, ENTY

SCALE: 1"=30' DATE: Jan 13, 202

PROPERTY OWNER PERMISSION

1, Louis and Shannon Froud, hereby permit the use of Motorcycle Dealership on my property at 2255 W. Hudson Rd		
on my property at 2255 W. Hudson Rd (use) Rogers, Arkansas.		
Dated this the 27 day of February, 2024.		
Ji In b		
Signed		
Louis Frond Shannon Froud		
Name Printed		
STATE OF ARKANSAS		
COUNTY OF White		
Subscribed and sworn before me this the 27 day of February, 2024.		
Manufacture of the state of the		
THER DAMME		
Notary Signature		
Heather Davis		
Heather Variation Notary Name Printed		
Exphasilities of the service of the		
Commission Expires Commission Expires		

CERTIFICATION

I hereby state that to the best of my knowledge all property owners within 300 feet of my property have been notified by Certified Mail **with return receipts requested** at least 15 days prior to the upcoming public hearing.

Dated this the 29 day of MARCH Signed	
WILLIAM E SULLVAN JA	2
Name Printed	
STATE OF TEXAS	
COUNTY OF <u>Denton</u>	
Subscribed and sworn before me this the	29 day of <u>March</u> , 20 <u>24</u> ,
GISELLE BUCH Notary Public, State of Texas Comm. Expires 06-02-2026 Notary ID 133791850	Notary Signature Giselle Buch Notary Name Printed
	Commission Expires



DEPT. OF COMMUNITY DEVELOPMENT
PLANNING DIVISION
301 W. CHESTNUT
PHONE: (479) 621-1186

FAX: (479) 986-6896

NOTICE OF PUBLIC HEARING

Notice is hereby given that a public hearing will be held before the Rogers Planning Commission in the Rogers City Hall at 301 West Chestnut Street on **April 16, 2024** at **5:30 p.m.** on the application by **William E. Sullivan** for a **Conditional Use Permit** to allow the use "Vehicle/Equipment Sales & Rentals" at 2255 W. Hudson Road in the C-2 (Highway Commercial) zoning district the following described location:

PROPERTY DESCRIPTION:

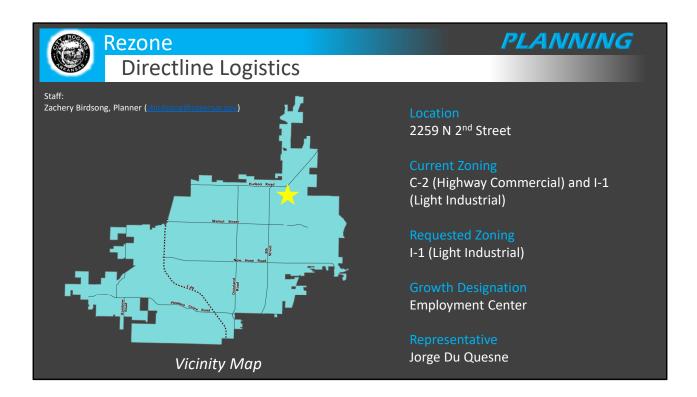
2255 W. Hudson Road

Parcel Number: 02-17521-000

Samantha Best, Secretary Planning Commission

PUBLISH ONE TIME ONLY: Sunday March 31, 2024

BILL THE CITY OF ROGERS



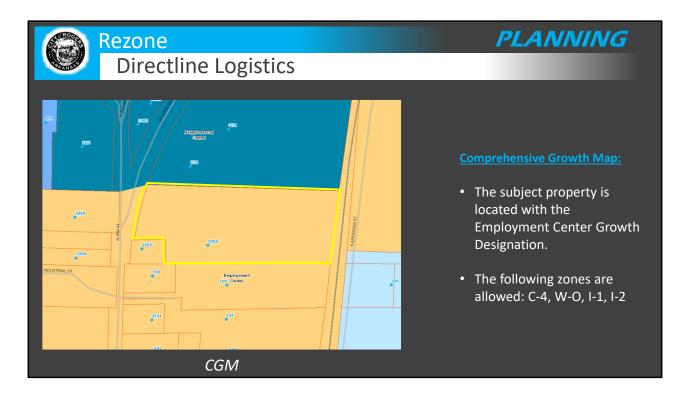
Summary:

This rezone request is for a property located in northeast Rogers at 2259 N 2nd Street.



GEOLOCATION:

The subject property is near the southeast corner of W Hudson Road and North 2nd Street.



General Findings:

- The subject property is located in the Employment Center Growth Designation.
- C-4, W-O, I-1, and I-2 zones are allowed within the Growth Designation.

Growth Designation:

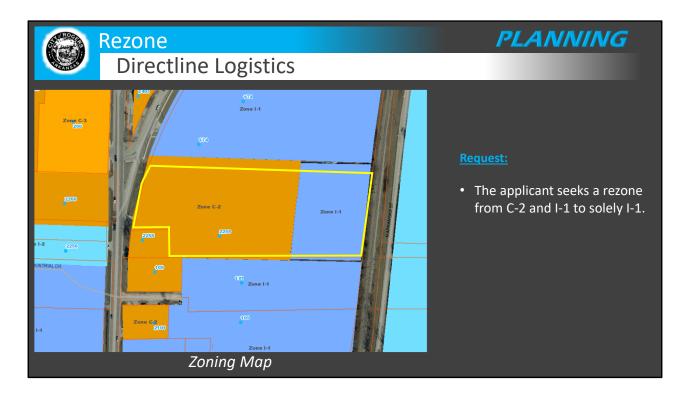
Employment Center

Growth Designation Purpose:

To maintain existing industrial activity while providing a location for activities like vehicle repair, construction yards, technology centers, and other physically and visually intense uses with high environmental impacts. (CGM Page 1).

Allowed Zoning Districts:

C-4, W-O, I-1, I-2



General Findings:

• The applicant is requesting to rezone the subject property from C-2 and I-1 solely to I-1.

Current Zoning:

- *C-2 (Highway Commercial):* "This district is intended for commercial uses, which depend upon high visibility, generate high traffic volumes or cater to the traveling public. These characteristics dictate that this district be located along or at the intersections of arterial classification streets or along frontage roads adjacent to the interstate or other limited-access streets." Sec. 14-709(a)
- *I-1 (Light Industrial):* "The I-1 light industrial district is intended to allow industrial operations and activities that do not create appreciable nuisances or hazards. It also is to provide areas within the City for the storage and transfer of goods. All industrial operations and activities are permitted provided they are conducted inside a building, although related outdoor storage and display are permitted. This district is situated close to heavy commercial and industrial areas and is intended to be located along rail lines and arterial and collector streets as identified by the master street plan." Sec. 14-713(a)

Requested Zoning:

• *I-1 (Light Industrial):* "The I-1 light industrial district is intended to allow industrial operations and activities that do not create appreciable nuisances or hazards. It also is to provide areas within the City for the storage and transfer of goods. All industrial operations and activities are permitted provided they are conducted inside a building, although related outdoor storage and display are permitted. This district is situated close to heavy commercial and industrial areas and is intended to be located along rail lines and arterial and collector streets as identified by the master street plan." Sec. 14-713(a)



General Findings:

- C-2 is not an allowed zone per the Employment Center Growth Designation.
- The requested zone, I-1, is allowed per the Comprehensive Growth Map.
- Adjacent properties are primarily industrial with some commercial uses.
- The request is compatible with the surrounding context.



Public Input Received:

• No public input has been received regarding this rezone request.

Recommendation:

Refer to committee due to there being more than one zoning option within this growth designation.



DEPT. OF COMMUNITY DEVELOPMENT PLANNING DIVISION CITY OF ROGERS, ARKANSAS 301 W. CHESTNUT

PHONE: (479) 621-1186 FAX: (479) 986-6896

OFFI	CE USE ONLY	
Permit Fee:		(\$200)
Zoning:	to	
Permit Number:		
CityView Application:		
Date:		

DIRECTLINE LOGISTICS LLC

REZONE APPLICATION

APPLICANT: DIRECTLINE LOGISTICS, LLC				
ADDRESS: 2259 N 2ND S	ADDRESS: 2259 N 2ND ST, ROGERS, AR 72756 SUITE #:			
GENERAL LOCATION OF	PROPERTY: 2ND ST AND I	HUDSON ROAD		
PARCEL NUMBER: 02-00	051-001			
PHONE #: 479-271-9360	EMAIL: steve	e@barrettdirectline.com		
PROPERTY OWNER: DIR	ECTLINE LOGISTICS, LLC	PHONE #: 479-271-9360		
PRESENT USE: Warehous	sing	ZONING: <u>C-2 / I-1</u>		
PROPOSED USE: Warehou	ısing	ZONING: I-1		
x la B	_ H	3-18-24		
Applicant Signature		Date		
Attachment Checklist: Legal description of property Parcel Number for property Applicant Certification Property Owner Affidavit Site plan as needed				
	PLANNING STAFF F	PROVIDES:		
DATE FILED:	PUBLIC HEARING DATE:	CERTIFIED MAIL DATE:		
PLANNING COMMISSION ACTION: DATE:				
CITY COUNCIL ACTION: DATE:				
ORDINANCE NUMBER: COMMENTS;				

PROPERTY OWNER AFFIDAVIT

The petitioner, DIRECTLINE LOGISTICS, LLC of Rogers, Arkansas to rezone certain real property	
LEGAL DESCRIPTION:	
See Attached	
LAYMAN'S DESCRIPTION:	
2259 N 2ND ST, ROGERS, AR 72756	
PRESENT ZONING: C-2 / I-1	
ZONING REQUEST:I-1	
	Respectfully Submitted, By: X
STATE OF ARKANSAS COUNTY OF Benton	
Subscribed and sworn before me this the 18 day	of March , 20 24 .
ONOTARIS S AUBLIC ONTER S ONTER S O	Notary Signature Puth Dwan Notary Name Printed 08 (4203) Commission Expires

CERTIFICATION

I hereby state that to the best of my knowledge all property owners within 300 feet of my property have been notified by Certified mail at least 15 days prior to the upcoming public hearing for my rezone request.

Dated this the day of March / PA	20 24
x (hy)2	
Signed	
Jorge Du Quesne	
Name Printed	
STATE OF ARKANSAS	
COUNTY OF BENTON	. 4
Subscribed and sworn before me this the	1st day of March April, 20 24.
,	Sulphu
THU A LE Notary Public - Arkansas Benton County	Notary Signature
Commission # 1272040 My Commission Expires Oct 25, 2033	THU A LE
	Notary Name Printed
	OCT 25, 2633
	Commission Expires



DEPT. OF COMMUNITY DEVELOPMENT PLANNING DIVISION 301 W. CHESTNUT PHONE: (479) 621-1186

FAX: (479) 986-6896

NOTICE OF PUBLIC HEARING

Notice is hereby given that a public hearing will be held before the Rogers Planning Commission in the Rogers City Hall at 301 West Chestnut Street on **April 16, 2024** at **5:30 p.m.** on the application by **Directline Logistics** to rezone **2259 N. 2nd Street** from the **C-2 & I-1** (Highway Commercial and Light Industrial) zoning districts to the **I-1** (Light Industrial) zoning district, the property being more particularly described as follows:

PROPERTY DESCRIPTION:

2259 N. 2nd Street

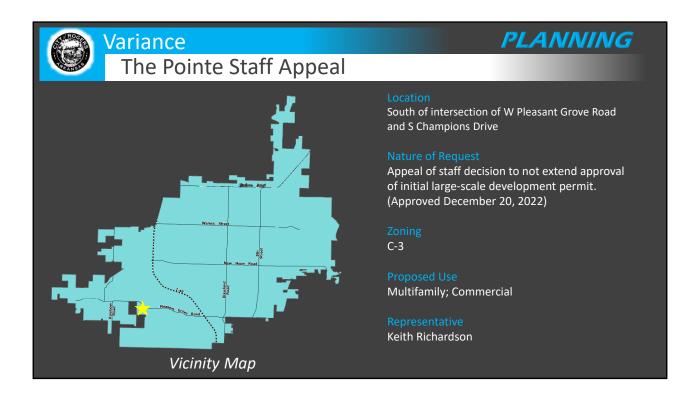
Parcel Number: 02-00051-001

Samantha Best, Secretary Planning Commission

PUBLISH ONE TIME ONLY: Sunday March 31, 2024

BILL THE CITY OF ROGERS





Summary:

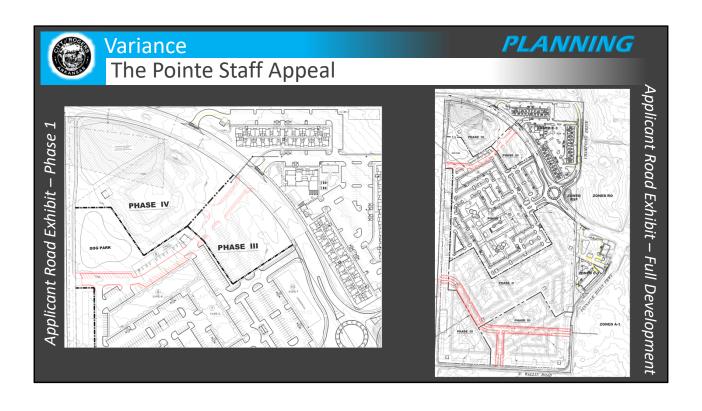
Applicant is appealing the staff decision to not extend LSDP approval, beyond the year time limit that is permitted. Sec. 14-228(35) states "Large-scale development plans are valid for one year following approval by the Planning Commission."

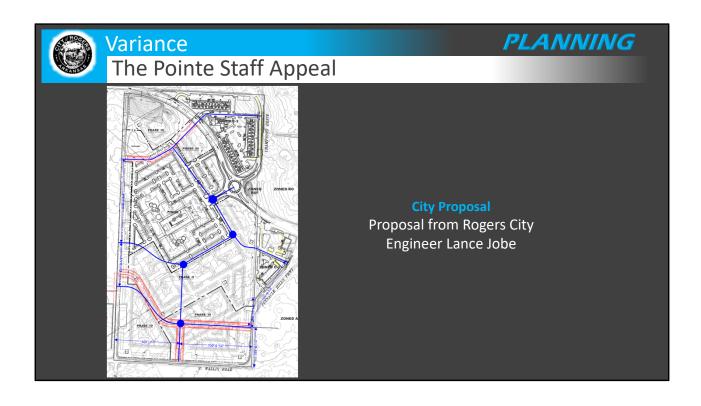
Growth Designation:

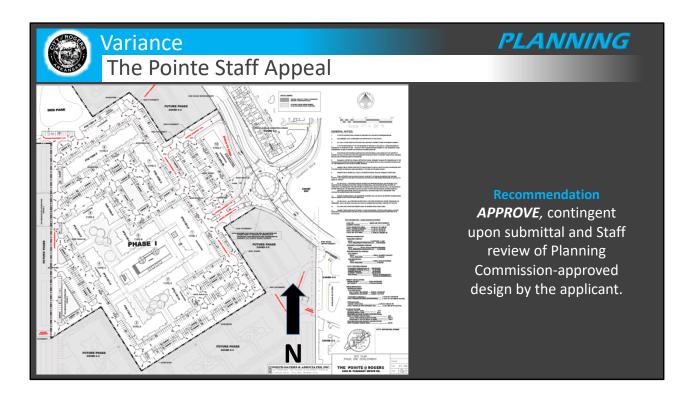
Neighborhood Center

Growth Designation Purpose:

To provide access to the essential activities of daily life within a walkable distance of surrounding residential areas in a way that promotes compact building and site design, encourages infill, and reduces vehicle miles traveled for basic needs.







Public Input:

No public input received.

Recommendation:

APPROVE, contingent upon submittal and Staff review of Planning Commission-approved design by the applicant.



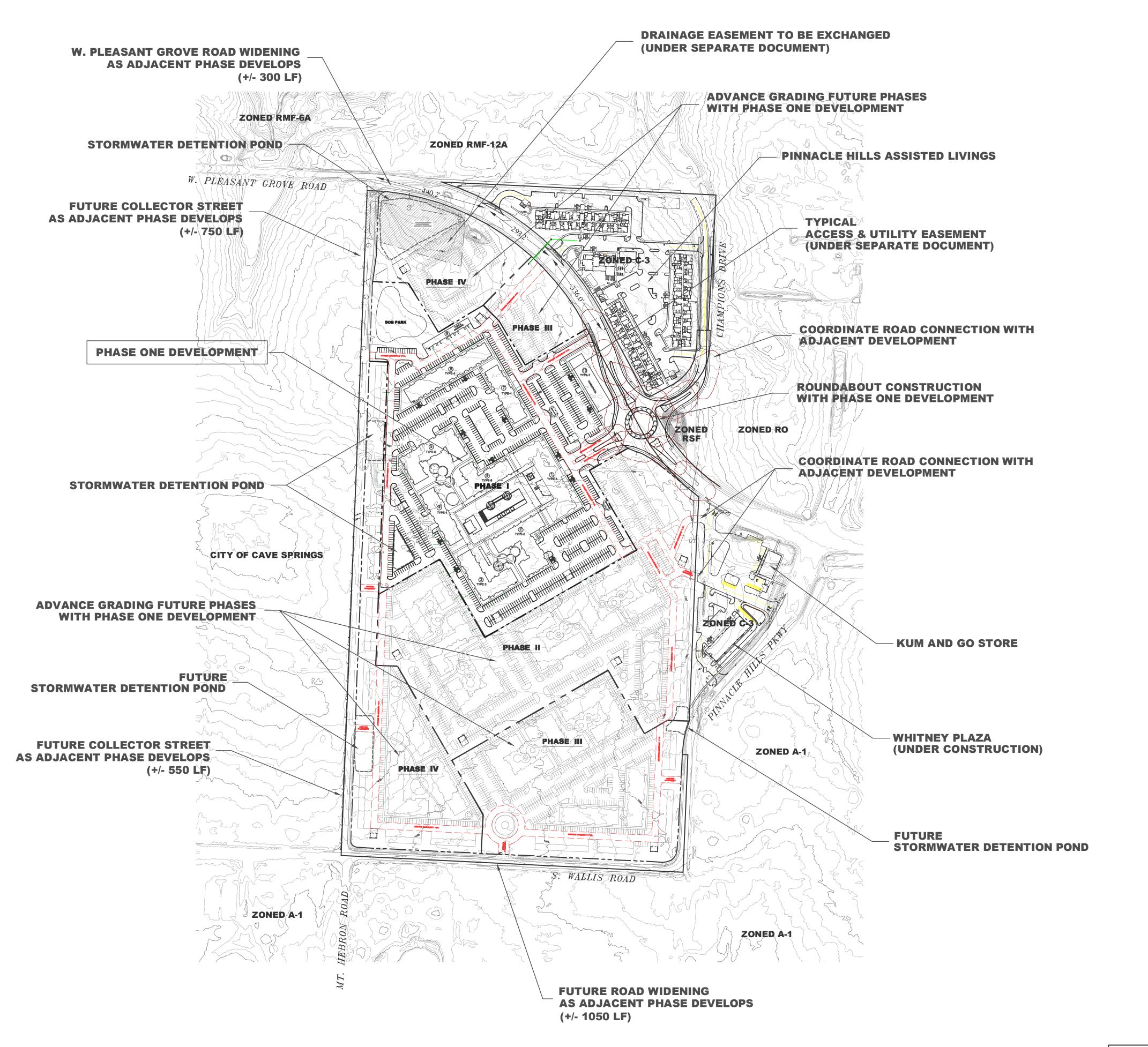
DEPT. OF COMMUNITY DEVELOPMENT PLANNING DIVISION

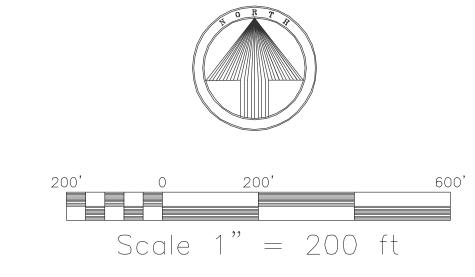
301 W. CHESTNUT PHONE: (479) 621-1186 FAX: (479) 986-6896

	OFFICE U	SE ONLY
Permit Fee:	(\$100)	Zoning:
App Number:		
CityView Applic	ation:	
Date:		

VARIANCE APPLICATION

APPLICANT: KEITH RICHARDSON,	THE POINTI	E AT ROGERS LP	
ADDRESS: 1902 S 8TH STREET. ROGER	as, AR 72758		SUITE #:
GENERAL LOCATION OF PROPERTY:	5,400 W. P.	LEASANT GROVE RD, RO	ODGERS, AR
PHONE #:501-350-9865	EMAIL:	keith@richsmithdev.com	1
PROPERTY OWNER: THE POINTE AT I	ROGERS LP	PHONE #:	501-350-9865
REQUEST to ALLOW(Cite Variation[s] from ROGERS PLANNING STAFF TO EXTEND THE A	APPRÔVAL OF	A LARGE SCALE DEVELOPM	ENT PLAN FOR 6 MONTHS.
Chaper 14, Article III, Division 2, Sec. 14-228 (35)	Large-scale deve	lopment plans are valid for one year followir	ng approval by the Planning Commission.
Sec. 14-724(e)(1)(b) requires variance ap consideration: How does strict enforcement of the code car property? THE APPLICANT REQUESTD AN EXTENSION OF	use undue har	dship due to circumstances	that are unique to this
DECLINED TO GRANT THE EXTENSION. TO DATE	E THE APPLICAN	IT HAS SPENT APPROXIMATLE	Y \$1,000,000 ON CONSTRUC TION
PLANS FOR THE DEVELOPMENT, SEWER EXTENSI	ONS AND PLAN	S FOR THE PROPOSED TRAFFIC	CIRCLE.
How will approval of this variance be in kee THE PREVIOULSY APPROVED LSDP WAS APPROV			
APPROVAL, MULTIPLE CONNECTIONS ARE SHOV	VN TO ADJACEN	T STREETS WITH DRIVES AND	WALKS WITH THE
DEVELOPMENT OPEN TO THE PUBLIC FOR INGRI	ESS AND EGRESS).	
The -		2-9-24	·
Applicant Signature		Date	
Attachment Checklist:			
Ճ Owner Signature on Letter or Applica			or above required responses
X Survey, elevations, sign proofs and/or	other docum	ents as needed to illustrate	request
	PLANNING STA	FF PROVIDES:	
BOARD OF ADJUSTMENT DECISION:			
SECRETARY, BOARD OF ADJUSTMENT		DATE of PI	UBLIC HEARING





NOTE: DRAWING SHOWN AT 200 SCALE FOR PURPOSE OF EXHIBITING ENTIRETY OF DEVELOPMENT.

CITY APPROVAL STAMP



WHITE-DATERS & ASSOCIATES, INC.

CIVIL ENGINEERING, LAND PLANNING & DEVELOPMENT, SURVEYING

Checked By:

CIVIL ENGINEERING, LAND PLANNING & DEVELOPMENT, SU A 24 RAHLING CIRCLE LITTLE ROCK, ARKANSAS 72223

Drawn By:

54

Approved By:

THE POINTE @ ROGERS
5400 W. PLEASANT GROVE RD.

SITE PLAN

OVERALL DEVELOPMENT

Date 01-04-24

Scale Hor. Vert.

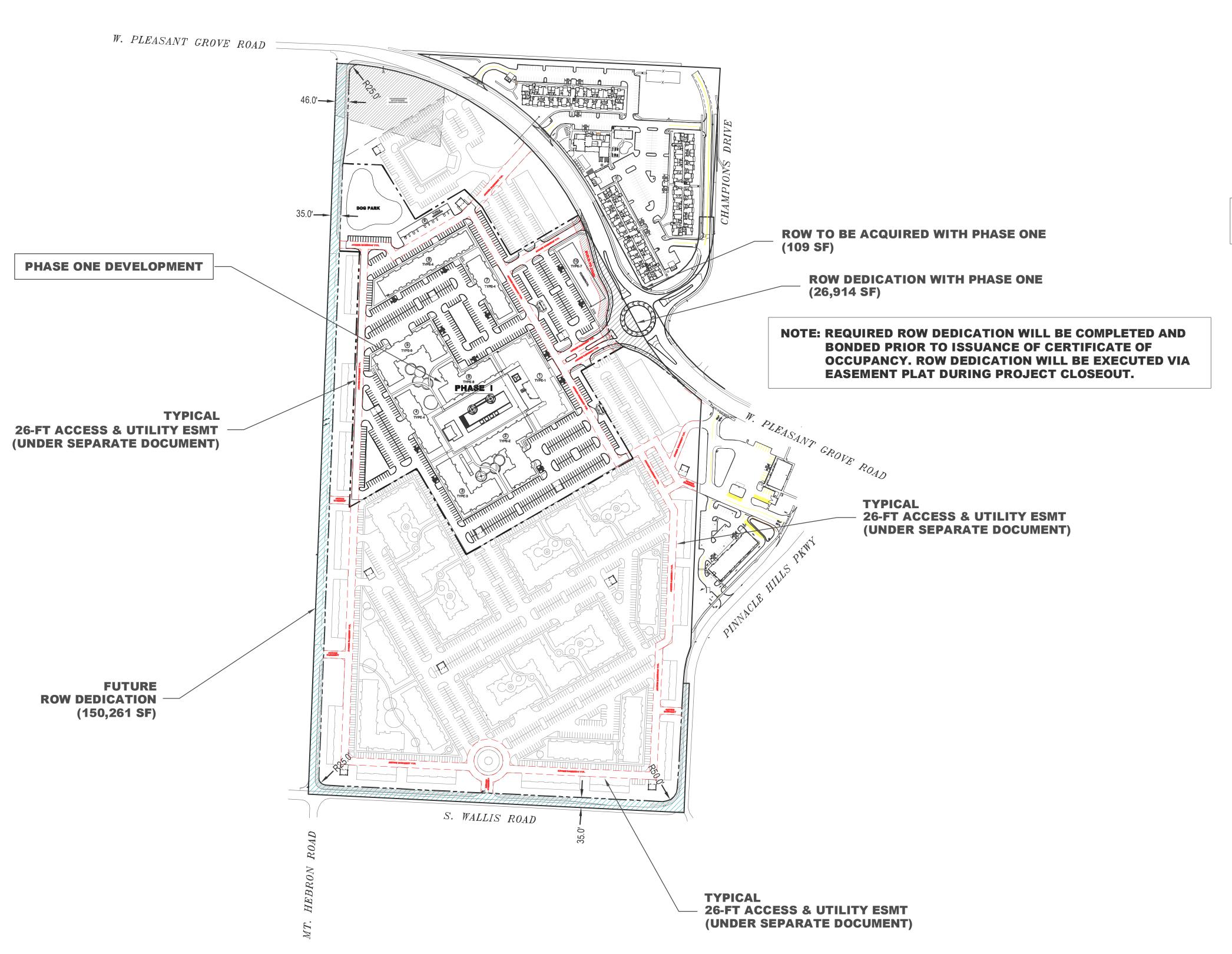
1"=200'

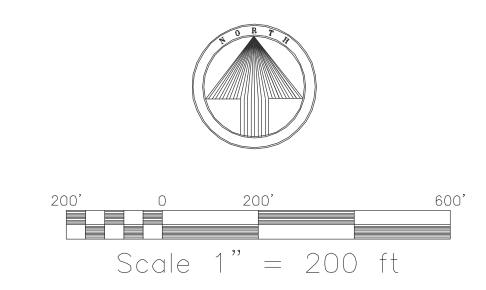
Sheet C01 of

Job No.

ROGERS, ARKANSAS

LSD23-0130





NOTE: DRAWING SHOWN AT 200 SCALE FOR PURPOSE OF EXHIBITING ENTIRETY OF DEVELOPMENT.

CITY APPROVAL STAMP



WHITE-DATERS & ASSOCIATES, INC.

CIVIL ENGINEERING, LAND PLANNING & DEVELOPMENT, SURVEYING

Checked By:

Approved By:

A 24 RAHLING CIRCLE LITTLE ROCK, ARKANSAS 72223

THE POINTE @ ROGERS
5400 W. PLEASANT GROVE RD.

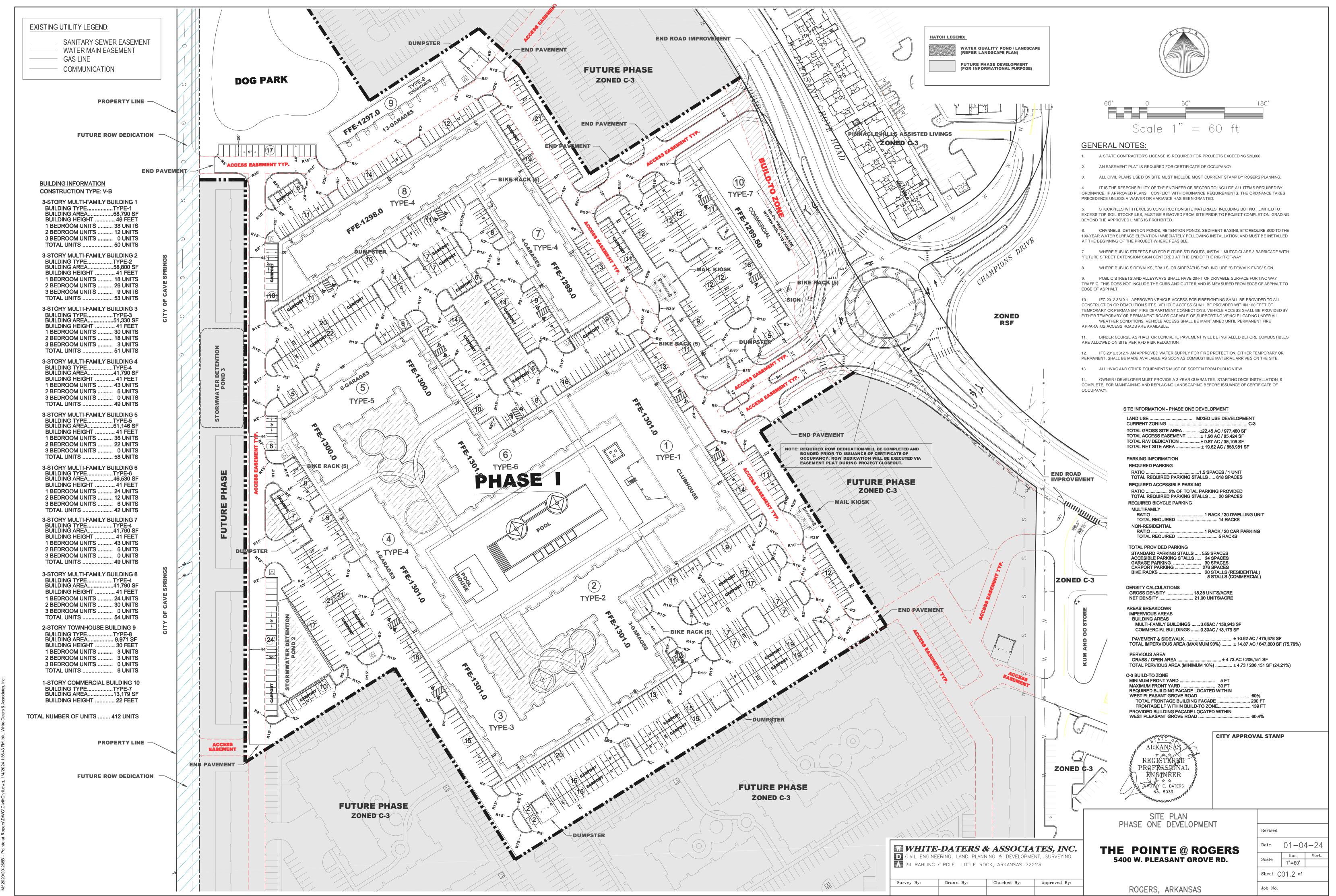
EXHIBIT RIGHT-OF-WAY DEDICATION

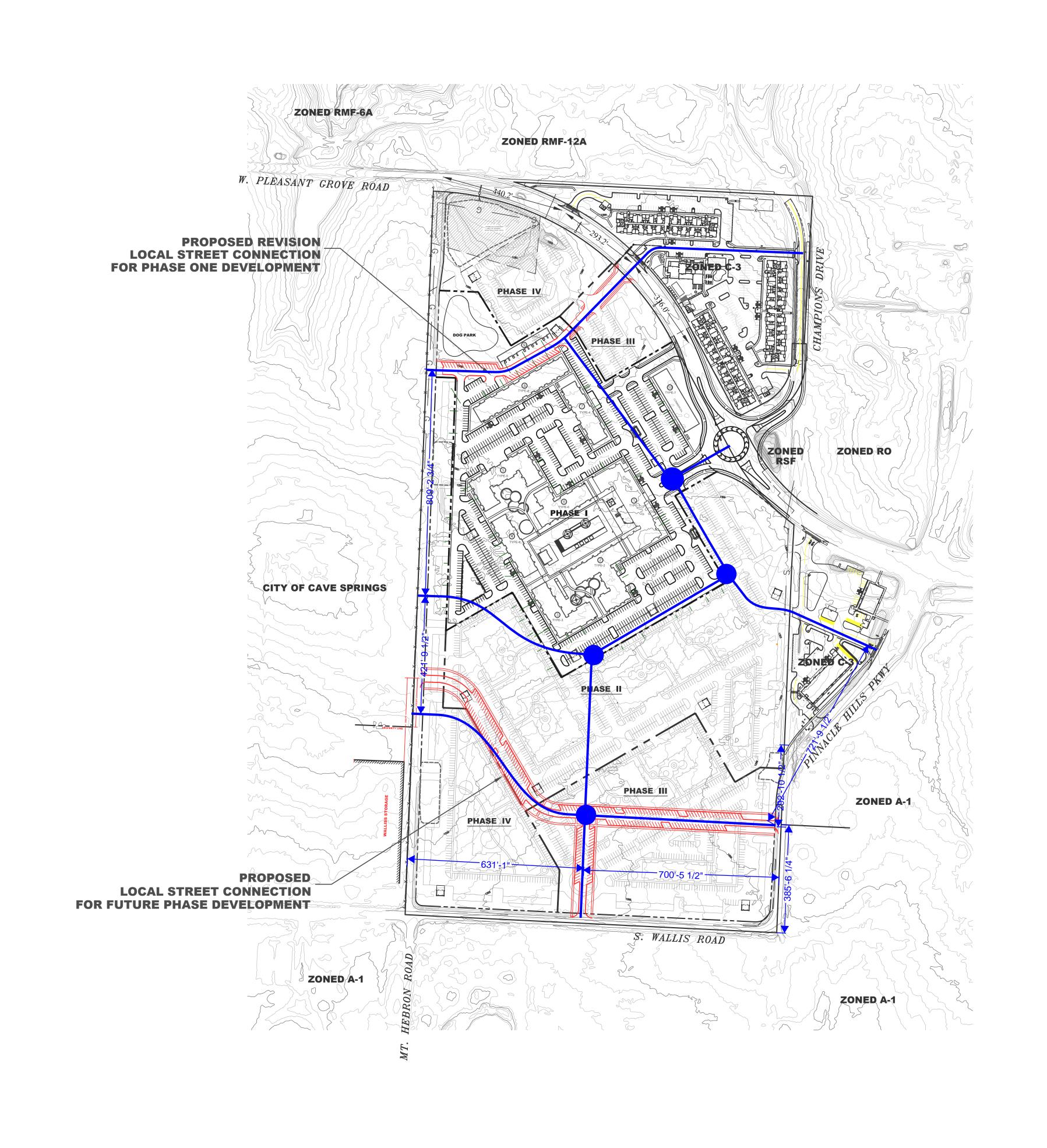
Date 01-04-24Scale Hor. Vert.

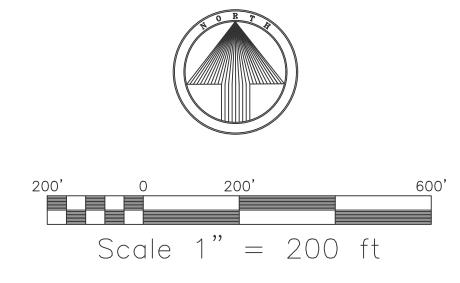
1"=200' Sheet CO1.1 of Job No.

ROGERS, ARKANSAS

LSD23-0130







CITY APPROVAL STAMP

W WHITE-DATERS & ASSOCIATES, INC.

D CIVIL ENGINEERING, LAND PLANNING & DEVELOPMENT, SURVEYING

A 24 RAHLING CIRCLE LITTLE ROCK, ARKANSAS 72223

Drawn By: Checked By: Approved By:

EXHIBIT — OVERALL MAP REVISION LOCAL STREET CONNECTION

THE POINTE @ ROGERS
5400 W. PLEASANT GROVE RD.

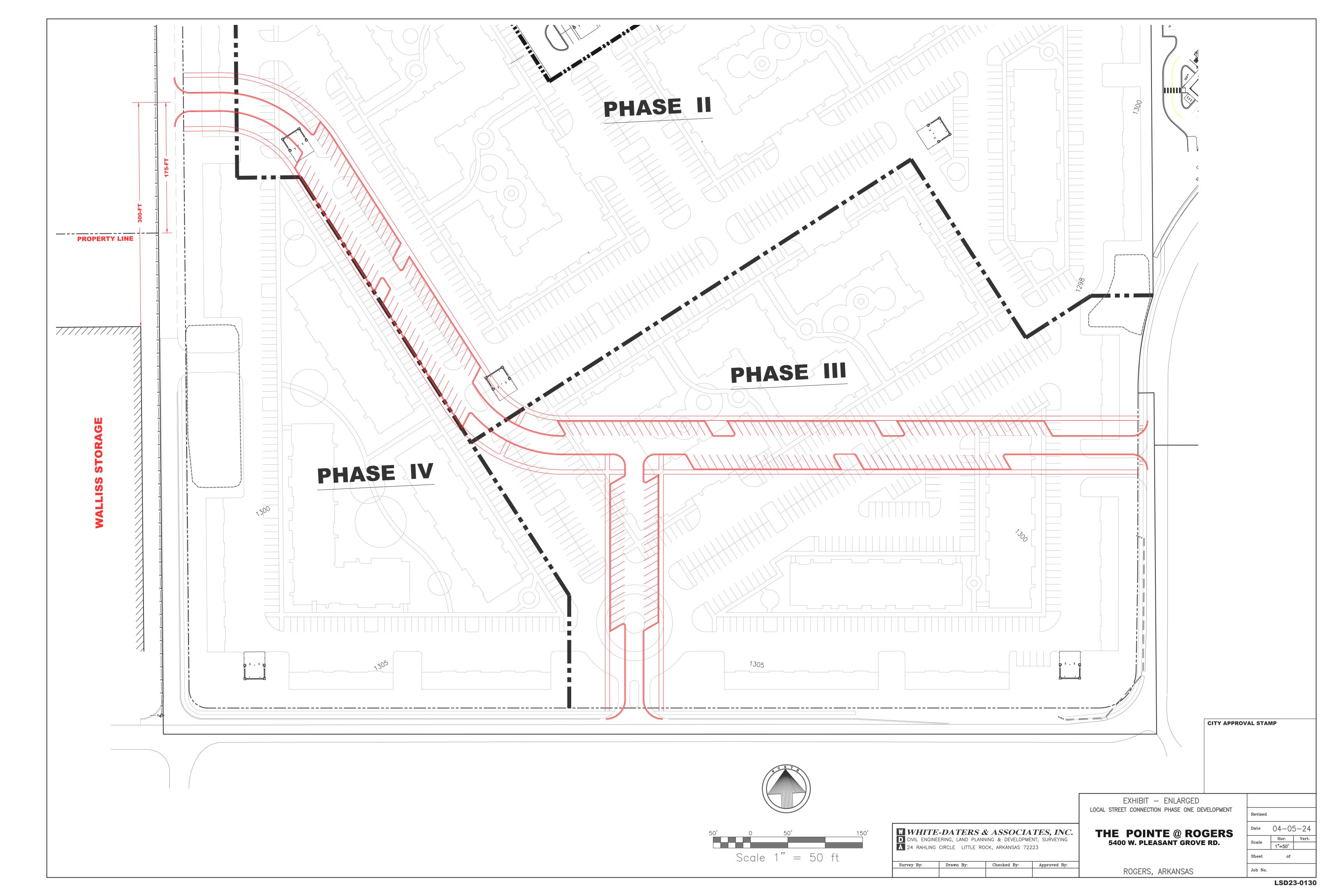
 Date
 04-05-24

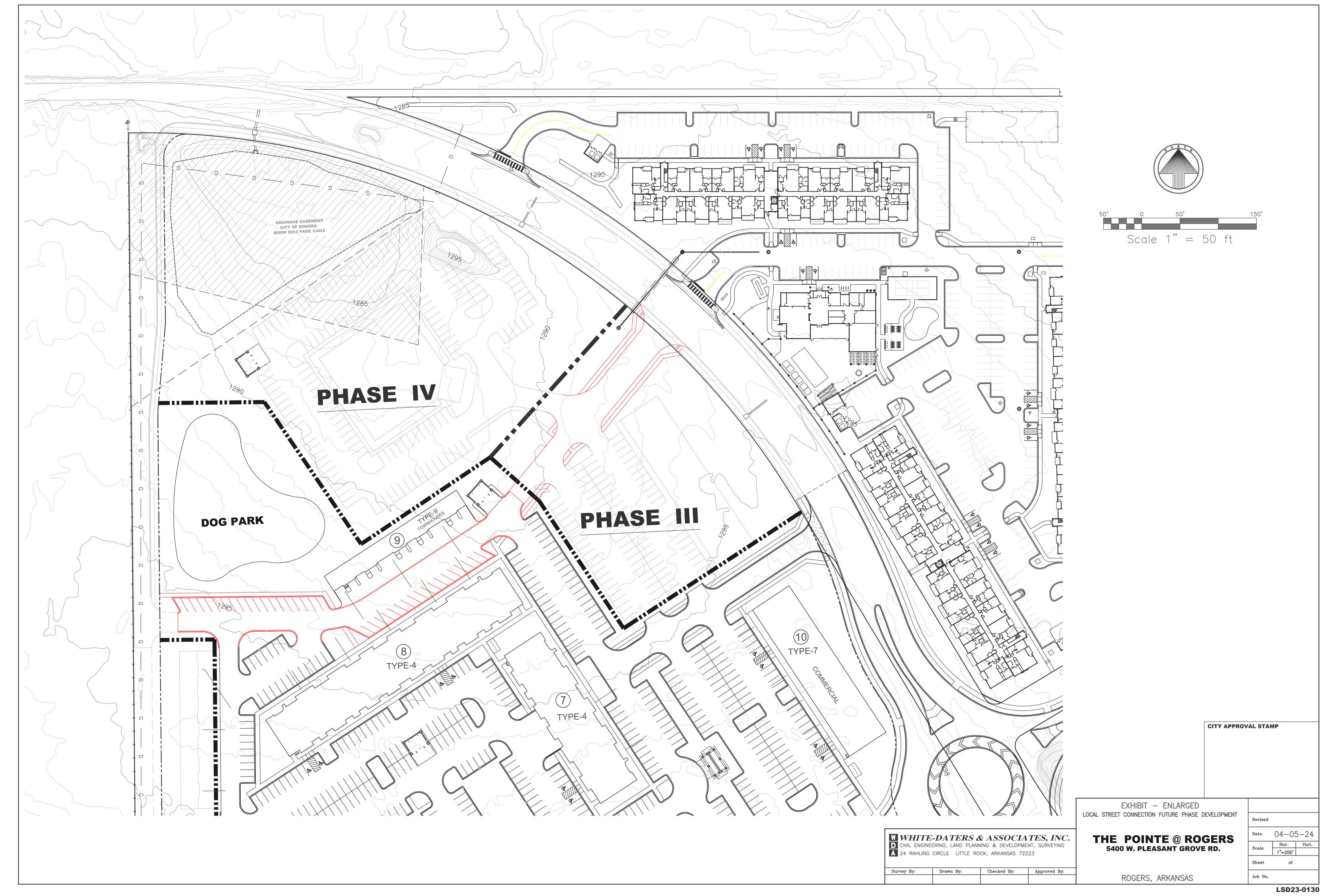
 Scale
 Hor.
 Vert.

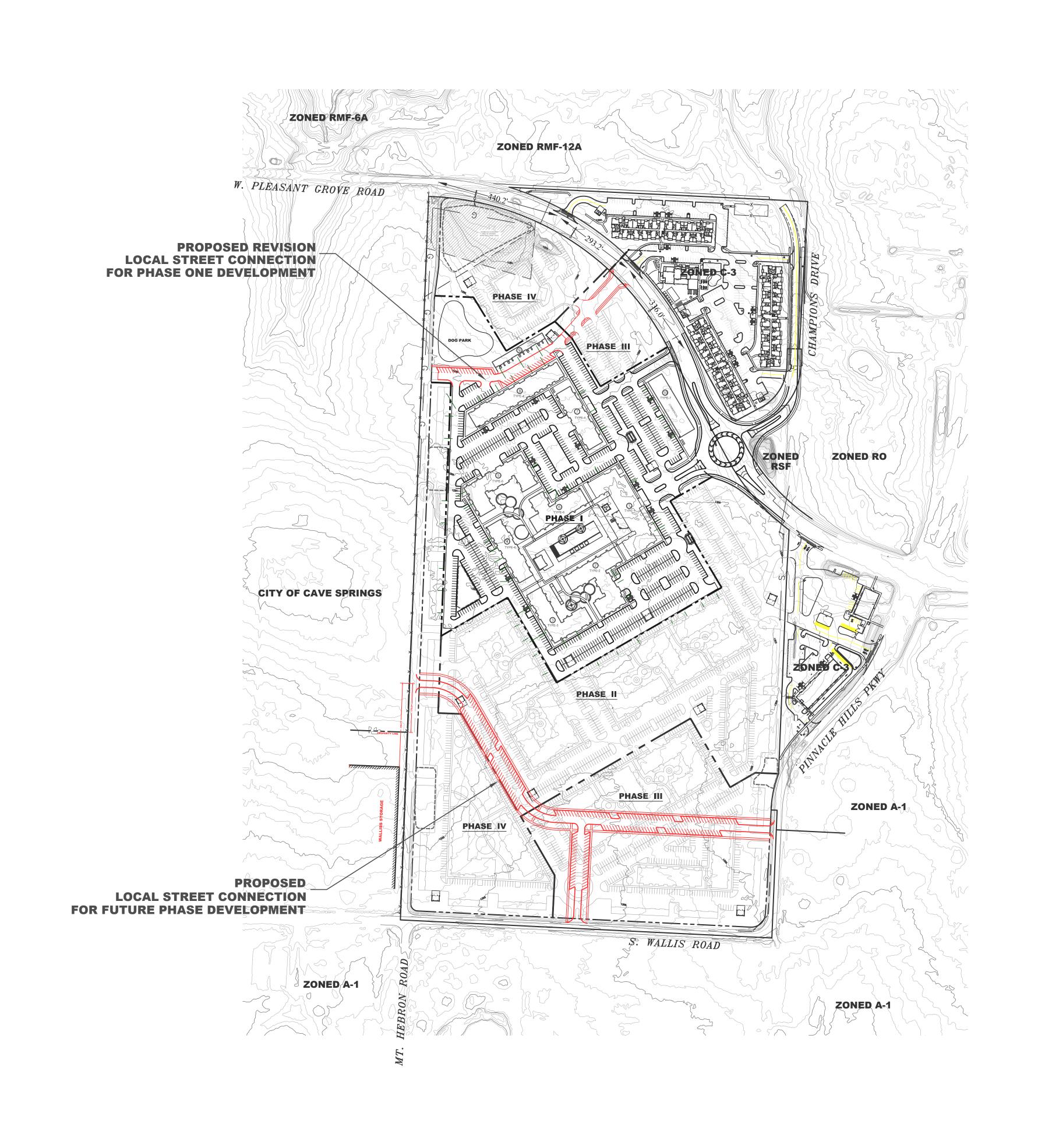
 1"=200'
 Sheet
 of

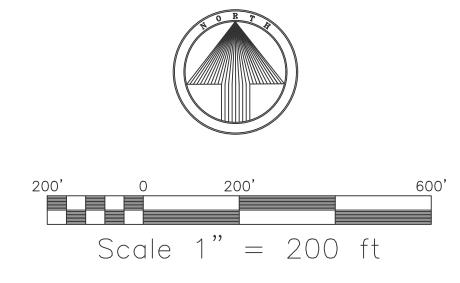
 Job No.
 Of
 Of

ROGERS, ARKANSAS









CITY APPROVAL STAMP

Approved By:

W WHITE-DATERS & ASSOCIATES, INC.

D CIVIL ENGINEERING, LAND PLANNING & DEVELOPMENT, SURVEYING A 24 RAHLING CIRCLE LITTLE ROCK, ARKANSAS 72223

Drawn By:

THE POINTE @ ROGERS 5400 W. PLEASANT GROVE RD.

EXHIBIT — OVERALL MAP REVISION LOCAL STREET CONNECTION

Date 04-05-24 Hor. Vert. Scale Sheet Job No.

ROGERS, ARKANSAS



DEPT. OF COMMUNITY DEVELOPMENT
PLANNING DIVISION
301 W. CHESTNUT
PHONE: (479) 621-1186

FAX: (479) 986-6896

NOTICE OF PUBLIC HEARING

Notice is hereby given that a public hearing will be held before the **Rogers Board of Adjustment** on **March 5, 2024** at **5:30 p.m.** at Rogers City Hall regarding an application by **The Pointe at Rogers** under the provisions of the City of Rogers Code of Ordinances, for a
variance to allow Rogers Planning Staff to extend the approval of the LSDP at ±67.17 acres **Near the intersection of S. Champions Drive and W. Pleasant Grove Road** in the C-3
(neighborhood commercial) zoning district more particularly described as follows:

PARCEL NUMBER:

02-02087-660

LOCATION:

±67.17 acres Near the intersection of S. Champions Drive and W. Pleasant Grove Road

Ed McClure, Secretary Board of Adjustment

PUBLISH ONE TIME ONLY: Sunday February 18, 2024

BILL THE CITY OF ROGERS



Location:

Located in north Rogers, north of W Hudson Road, directly adjacent to Little Flock city limits on it's east and north sides.

Proposal:

The request at hand proposes the use of vehicle storage on a gravel lot. Sec. 14-256(6) states that a waiver can be requested for the paving of parking within a development, if the parking is intended for storage of a vehicle and is screened from public view.



Planning Review:

COMPREHENSIVE GROWTH MAP:

Growth Designation:

Employment Center

Growth Designation Character:

Single-use areas in proximity to major arterials and rail intended for industrial and commercial uses with substantial site activity. Physical separation and landscape screening necessary for compatibility with adjacent areas. Goals include responsibly organizing uses with like impacts and maintaining capacity for industry and service. (CGM PAGE 1)

Allowed Zoning Districts:

C-4, W-O, I-1, I-2

ZONING:

I-1 (Light Industrial):

The I-1 light industrial district is intended to allow industrial operations and activities that do not create appreciable nuisances or hazards. It also is to provide areas within the City for the storage and transfer of goods. All industrial operations and activities are permitted provided they are conducted inside a building, although related outdoor storage and display are permitted. This district is situated close to heavy commercial and industrial areas and is intended to be located along rail lines and arterial and collector streets as identified by the master street plan.

Use Definition:

- 1. Vehicle Storage is a secondary use classification encompassing exclusive storage of operative or inoperative vehicles;
- 2. Vehicle Storage includes without limitation the tertiary use classifications of:
 - i. Storage of parking tow-aways;
 - ii. Impound yards; and
 - iii. Storage lots for automobiles, trucks, buses, and recreational vehicles.
- 3. Vehicle storage does not include vehicle dismantling.

Sec. 14-695(b)(3)(nn)



PLANNING

Regulating Code

- Section 14-256. Design standards, regulations and required improvements
 - (6) Parking must meet the requirements of the zoning ordinance and must be paved. However, the Planning Commission may upon request waive the paving requirement under this subdivision if the parking is intended for storage of a vehicle and is screened from public view. Handicap spaces provided must meet local, state and ADA requirements, including truncated domes at all handicap ramps.

	WAIVER/LSD	P	PLANNING
	NWA Drop	Yard	
PROJECT NAME, Wishar, In ADDRESS/LOCATION: 2609 PROPERTY OWNED BY: Mit ADDRESS; 325 A 45th Steet CONTRACTOR'S NAME: \$3.8 ADDRESS; 1922 Prieseods ENGINEER'S NAME: \$4.8 ADDRESS; 1922 Prieseods ADDRESS; P.O. BOY \$464, E STATE OF ARKANSAS NOI P APPLICATION DATE: 8172 ATTACHI AND OI APPLICATION: TATACHI PLANNING COMMISSION AF STAFF ACTION: APPLICATION: APPLICATION: APPLICATION FEE: PERMIT EXPIRES IF NO ACC	W. Hudson Road, Rogers, AR 72758 hat, Inc. TELEPHONE NUMBER: 479-986-0311, .Rogers, AR 72758 LOT TELEPHONE NUMBER: 479-986-0311, .Rogers, AR 72758 Dr. Rogers, AR 72758 Dr. Rogers, AR 72758 LOT TELEPHONE NUMBER: 479-351-5529 Dr. Rogers, AR 72714 LOT TELEPHONE NUMBER: 479-367-2924 LOT TELEPHONE NUMBER: 499-367-2924 LOT TELEPHONE NUMBER: 499-367-2924 LOT TELEPHONE NUMBER: 499-367-2924 LOT TELEPHONE NUMBER: 499-367-367-367-367-367-367-367-367-367-367	 Grading Permit Grading permit issued 8/24/2020, according to city records. Requirement for grading permit came about after Community Development staff was notified of clearing and equipment/material storage on the site. 	MANDEM - 1975

Use Permit

- 2020-172
- Use permit approved on 7/29/2020 for the use "Building Materials and Services"
- Sec. 14-695(b)(3)(j) Building Materials is a secondary use classification encompassing retail, wholesale, or rental of building supplies, equipment, or services.
 - 2. Building Materials includes but is not limited to the following tertiary use classifications:
 - i. Lumber yards;
 - ii. Tool and equipment sales;
 - iii. Tool and equipment rental; and
 - iv. Building contractors' yards.

Grading Permit

- Issued 8/24/2020
- Only allowed grading of the south 4.95 acres of the property, including the driveway that reaches down to W Hudson Road.
- Gravel lot has since expanded to the north beyond what was permitted, without another grading permit



Clearing without a Permit

It appears that between early 2020 (The time in which the aerial imagery that City of Rogers GIS uses was captured) and early 2021, most of the existing trees and vegetation were cleared.

A grading permit was issued on 8/24/2020 by Community Development. The permit seems to only be issued for the grading of 4.95 acres, which seems to be the bounds of the graveled area in the 2021 imagery. The graveled area was expanded sometime between early 2021 and early 2022, according to aerial imagery. Community Development staff has no records of receiving or approving conceptual plans or permits during this time from the applicant.

Despite the grading plan only showing less than 5 acres, nearly the entire site (19.9 acres) was clear-cut. The owner of the property states that the site will be developed at some point, but Community Development staff has not received any large-scale plans showing a full development – only one paving the driveway from Hudson and turnaround area on the south side of the gated entrance into the Drop Yard. Until staff receives a formal concept and guarantee that the entire site will be built out in an agreed-upon time frame, the property owner will be expected to replace the trees that were cleared outside of the development area at a 5:1 ratio. (5 trees planted for every 1 removed)

There has been no expansion of the graveled area since the last hearing, on 8/15/2024.



PLANNING

Staff Findings

- Vehicle Storage is a permitted use in I-1 zoning.
- Sec. 14-256(6) states "the Planning Commission may upon request waive the paving requirement under this subdivision if the parking is intended for storage of a vehicle and is screened from public view.
- Intent of screening language in this portion of the code is to shield adjacent properties from the dust that gravel creates when driven on.
- Section of code that requires waiver for vehicle storage on gravel DOES NOT permit or allow other uses on gravel, like "Building Materials and Storage"
- Adjacent neighbors to the east of the subject property are currently used as single-family residential.

Staff Findings

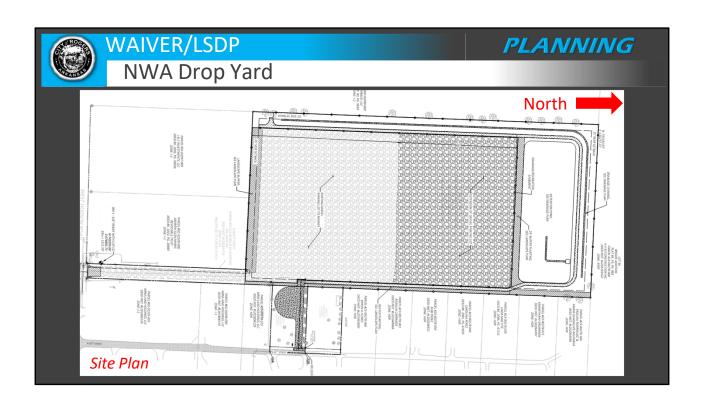
- Vehicle Storage is a permitted use in I-1 zoning.
- Sec. 14-256(6) states "the Planning Commission may upon request waive the paving requirement under this subdivision if the parking is intended for storage of a vehicle and <u>is</u> screened from public view.
- Intent of screening language in this portion of the code is to shield adjacent properties from the dust that gravel creates when driven on.
- Adjacent neighbors to the east of the subject property are currently used as single-family residential.
- Gravel lot was expanded, existing trees were cleared, and vehicle storage has been taking place here, all without proper permitting.
- Section of code that requires waiver for vehicle

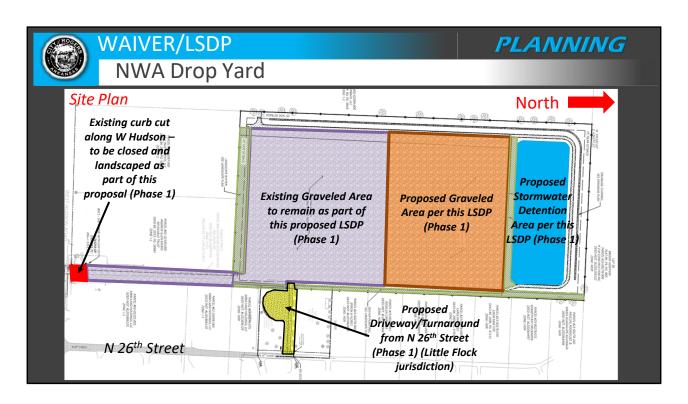


*****Yellow highlighted area represents parcels purchased by subject property owner, to be developed as a driveway and turnaround for proposed development.

Adjacencies

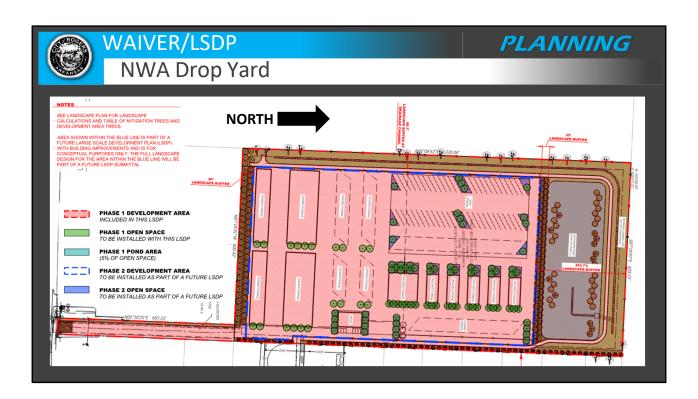
- South Rogers Industrial/Open display commercial uses
- West Rogers Industrial/Chemical/Storage Use
- North Little Flock Undeveloped Ag. Currently... zoned for low-density residential
- West Little Flock Mostly single-family residential currently... Mixed zonings (Commercial, Agricultural, Single-family residential, Multifamily residential)
- NOTE: The shown Little Flock zoning map has not been adopted, as of 8/15/2023.
- According to applicant's correspondence with Kim Kendrick of City of Little Flock, the two parcels proposed to be developed by the applicants are zoned commercial. (March 4, 2024)





The exhibit above describes different aspects of the proposed development.

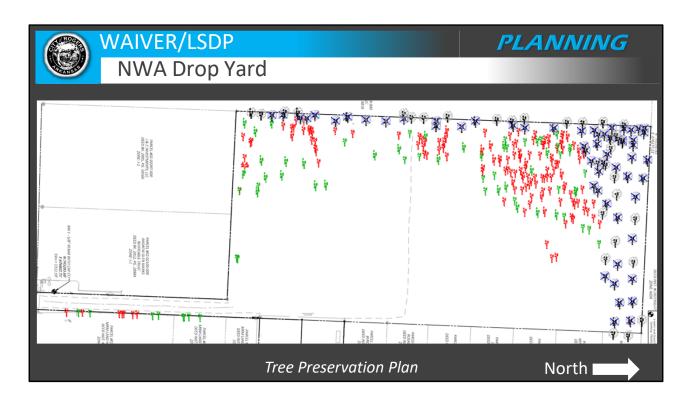
- Purple Existing Graveled Area to remain
- Orange Proposed Gravel Extension
- Blue Proposed Stormwater Detention area
- Yellow Proposed Driveway from N 26th Street
- Red Proposed closed curb cut along Hudson; to be landscaped as part of this proposal.
- Green Proposed landscaping and bufferyard areas.



FUTURE PLAN / PHASING PLAN

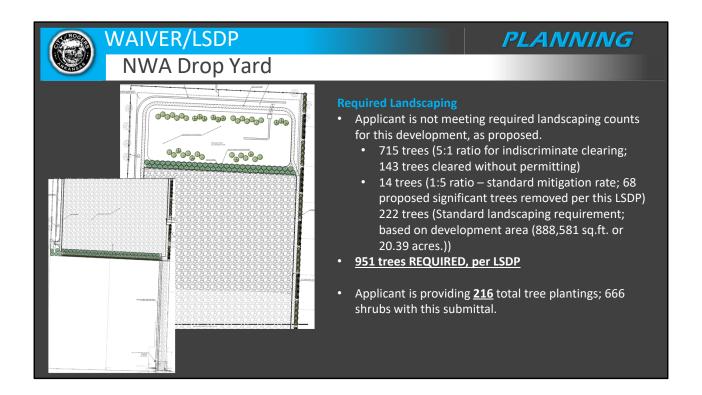
As part of this LSDP, applicants are proposing the shown phasing plan. Staff has no way of requiring future phases of development to be built out beyond approval of the first phase.

- Red-Dashed Area is proposed phase 1. Includes full graveling of site, new
 driveway from Little Flock, stormwater detention pond on north end(light
 blue), greenspace/landscape buffer areas at existing curb cut on W Hudson
 Road, landscape buffer areas along all sides of development, and landscaping
 between graveled area and stormwater detention.
- Phase 2 area is outlined in blue dash, including full paving of the shown area, and built out with covered and enclosed storage areas, tree islands and internal landscaping.



Tree Preservation Plan

- Dated 4/2/2024
- Red dots signify significant trees cleared (8" or greater in caliper) 143
 Significant trees removed from the site
- Green dots signify insignificant trees cleared (Less than 8" in caliper)
- Trees with X's indicate significant trees to be cleared as part of the proposed LSDP
- Tree outline with no X indicate preserved trees., per proposed LSDP
- 5:1 tree mitigation ratio(applied to trees cleared without permitting or cleared outside of development area) would require the developer to plant 715 new trees. Standard 1:5 mitigation ratio applied to trees removed because of development. In this instance, 14 standard mitigation trees would be required, totaling 729 mitigation trees for the entire site. This number does not include required landscaping provisions, based off of total development area.



LANDSCAPING

- Total Trees Required: 715(5:1 ratio for indiscriminate clearing) + 14(1:5 standard mitigation ratio for significant trees removed within proposed development area) + 222(Standard required landscaping trees based on development area) = 951 trees REQUIRED
 - 216 trees PROVIDED with this LSDP



PLANNING

Outstanding Comments

RWU

- Large trees (over 20' tall at maturity in City Code 14-46(g) must be located at least 10' from any water or sanitary sewer main.
- Is this an accurate depiction of the existing edge of gravel?

Engineering

- Indicate that the curb cut on Hudson Drive is to be removed.
- Provide street trees on Hudson Road in accordance with Section 14-46 (g). If there are conflicts with ROW, provide street trees within a recreation easement behind the ROW.
- Trees cleared without proper permitting or approval are subject to a 5:1 replacement ratio. The proposed alternative compliance ratio is not sufficient.

Engineering (cont.)

- Reference to Sec. 14-796(c)(1-4)
- Add note: Large-scale development plans are valid for one year following Planning Commission approval.
- Show any trees that are proposed to be removed on the Little Flock parcels. Ensure that these are included in the mitigation calculations.
- Indicate curb, gutter, and sidewalk will be reconstructed on Hudson Road
- Clearly indicate the boundaries of proposed drainage easements
- Indicate the location of construction entrance, concrete washout, ADEQ mailbox. These are all required at the beginning of construction.
- Indicate the location of erosion control BMPs such as silt fence, wattle, etc. This is required to be constructed at the beginning of construction.
- Include Minor Street Typical Street Section for 26th
 Street. Design should generally meet the requirements outlined in the Street Section.

RWU

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generally meet the requirements outlined in the Street Section.



PLANNING

Outstanding Comments

· Engineering (cont.)

- Provide pedestrian access from ROW.
- Provide turning templates for 18-wheelers accessing the 26th Street driveway.
- 26th Street requires full reconstruction per the Typical
 Street Section, not a mill and overlay. Update legend.
- Construct curb and gutter on 26th Street. Add to cross section.
- Provide profile of 26th Street to verify drainage requirements can be met.
- Include N 26th Street reconstruction in limits of disturbance.
- Call out radius of intersection of 26th and Hudson.
- Provide crosswalk, ADA ramps and stop bar at intersection.
- Provide a striping plan for the intersection.
- Indicate what constitutes the landscape buffer as referenced on sheet C2.02.

Planning

- List if trees are shade, ornamental or evergreen trees
- Show height of all proposed tree plantings
- Use of gravel is subject to Planning Commission waiver approval for the use "vehicle storage."

Later reviews of subsequent submittals may generate comments that are not in this list.

RWU

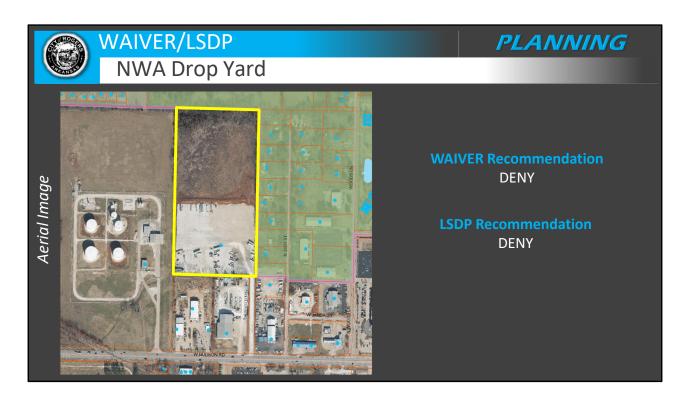
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- Is this an accurate depiction of the existing edge of gravel?

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- Provide street trees on Hudson Road in accordance with Section 14-46 (g). If there are conflicts with ROW, provide street trees within a recreation easement behind the ROW.
- Trees cleared without proper permitting or approval are subject to a 5:1 replacement ratio. The proposed alternative compliance ratio is not sufficient.
- Reference to Sec. 14-796(c)(1-4)
- Add note: Large-scale development plans are valid for one year following Planning Commission approval.
- Show any trees that are proposed to be removed on the Little Flock parcels. Ensure that these are included in the mitigation calculations.
- Indicate curb, gutter, and sidewalk will be reconstructed on Hudson Road
- Clearly indicate the boundaries of proposed drainage easements
- Indicate the location of construction entrance, concrete washout, ADEQ mailbox. These are all required at the beginning of construction.
- Indicate the location of erosion control BMPs such as silt fence, wattle, etc. This is required to be constructed at the beginning of construction.
- Include Minor Street Typical Street Section for 26th Street. Design should generally meet the requirements outlined in the Street Section.

Planning

- List if trees are shade, ornamental or evergreen trees
- Show height of all proposed tree plantings
- Use of gravel is subject to Planning Commission waiver approval for the use "vehicle storage."



<u>WAIVER:</u> Because of there not being a site-specific hardship for NOT paving the storage lot, an existing illegal use taking place on the property currently (building & material storage <u>on gravel</u>, and because of the subject property owner's past behaviors, Staff recommends **DENIAL** of the requested waiver.

<u>LSDP:</u> The proposed LSDP shows the paved, fully built out design as Phase 2 of the entire development, with Phase 1 being expansion of the existing gravel lot. If approved, the developer would not be required to build out the second phase, just because of Phase 1's acceptance. Because of this, Staff recommends **DENIAL** of the LSDP request as is.

Chad Caletka

From: Kim Kendrick <kkendrick@cityoflittleflock.com>

Sent: Wednesday, March 6, 2024 1:56 PM

To: Chad Caletka

Cc: Billy Witcofski; Nate L. Bachelor **Subject:** Re: Business Entrance in Little Flock

Per conversation with Mayor V and our street superintendent, you will not need a permit from Little Flock. It looks like most of N 26th will be permitted by Rogers, so we feel confident you will be following rules on building a street. The work on your property is under one acre and will not need a permit if all you're doing is building a road and no buildings. Once you decide how to proceed with getting rid of the old building (burn or demo), fill out a permit for this. Thank you.

Thank you,

Kim Kendrick

Planning & Zoning Secretary/Administrator

City of Little Flock 479-636-2081 opt. 2 City Hall 479-636-2318 Fax Hours: 8a-4p M-F

On Tue, Mar 5, 2024 at 9:28 AM Kim Kendrick < kkendrick@cityoflittleflock.com> wrote: Ok, thanks. I'll get back to you.

On Tue, Mar 5, 2024 at 9:27 AM Chad Caletka <ccaletka@ceieng.com> wrote:

Thank you Kim. Yes, we are intending to pave 26th from Hudson up to the proposed driveway on the property.

From: Kim Kendrick <kkendrick@cityoflittleflock.com>

Sent: Monday, March 4, 2024 4:46 PM **To:** Chad Caletka < ccaletka@ceieng.com **Subject:** Re: Business Entrance in Little Flock

Chad.

The parcels in Little Flock are zoned commercial. I'll show your site plan to a commissioner to follow up on your other question. Are you covering all of N 26th up to your entrance or just what is in front of your parcel or just what is on your parcel?

Thank you,

Kim Kendrick **Planning & Zoning Secretary/Administrator** City of Little Flock 479-636-2081 opt. 2 City Hall 479-636-2318 Fax Hours: 8a-4p M-F On Mon, Mar 4, 2024 at 3:52 PM Chad Caletka < ccaletka@ceieng.com > wrote: Good afternoon Kim, do you have an update on the status of the request below? I'm available on my cell if it's easier to discuss on the phone, (479)321-0531. Thank you, Chad From: Chad Caletka Sent: Monday, February 19, 2024 12:31 PM To: 'kkendrick@cityoflittleflock.com' <kkendrick@cityoflittleflock.com> Cc: Nate L. Bachelor <nbachelor@ceieng.com>; Billy Witcofski <billy@wilshar.com> **Subject:** Business Entrance in Little Flock Good afternoon Kim, We are working for Mr. Witcofski on improvements to the Drop Yard NWA site (Rogers, Parcel # 02-02100-002) and would like to build a new driveway to access the property; the proposed driveway is on the site of Little Flock parcel 23-00278-070 at 2808 N 26th Street. We have two questions: 1. Please verify the zoning for Little Flock parcels 23-00278-065 and 23-00278-070.

what Little Flock permitting process will be required to permit the driveway construction.

2. Confirm if the commercial driveway improvements on Parcel 23-00278-070 are allowed by right, or

I have attached a site plan for the proposed improvements to access Parcel **02-02100-002** via the driveway off N 26th Street. The improvements to Parcel **02-02100-002** are under review with City of Rogers for the commercial site.

Reach out with any questions or if you need further information.

Thank you,



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Thank you,

Kim Kendrick

Planning & Zoning Secretary/Administrator
City of Little Flock
479-636-2081 opt. 2 City Hall
479-636-2318 Fax
Hours: 8a-4p M-F

D)R(O)P) Y/A\R(D) |\\\/A\

C/17YW/EWPROJECT#PL20230032

2609 WEST HUDSON ROAD

. HUDSON RD. - HWY 62

CEI CONTACT:

CHAD CALETKA

(479) 273-9472

(479) 273-9472

DEREK HILEMAN

PROJECT MANAGER

CCALETKA@CEIENG.COM

ENGINEER OF RECORD

NBACHELOR@CEIENG.COM

CONSTRUCTION MANAGER

DHILEMAN@CEIENG.COM

ROGERS, ARKANSAS

GENERAL NOTES:

A. TOPOGRAPHIC BOUNDARY SURVEY, INCLUDING PROPERTY LINES, LEGAL DESCRIPTION, EXISTING UTILITIES, SITE TOPOGRAPHY WITH SPOT ELEVATIONS, OUTSTANDING PHYSICAL FEATURES AND EXISTING STRUCTURE LOCATIONS WAS PROVIDED BY THE FOLLOWING COMPANY, AS A CONTRACTOR TO THE SELLER/OWNER:

TOPOGRAPHY: CASTER & ASSOCIATES LAND SURVEYING, INC. 2715 SE "I" STREET, SUITE 5 BENTONVILLE, AR 72712 (479) 268-4464

> CEI ENGINEERING ASSOCIATES. INC 3108 SW REGENCY PARKWAY, SUITE 2 BENTONVILLE, AR 72712 (479) 273-9472

- B. ALL PHASES OF SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE OWNER / DEVELOPER SITE WORK SPECIFICATIONS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF EXISTING STRUCTURES. RELATED UTILITIES, PAVING, UNDERGROUND STORAGE TANKS AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED. SEE SITE WORK SPECIFICATIONS.
- CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL **REGULATIONS GOVERNING SUCH OPERATIONS**
- THE GENERAL CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR AND SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- WARRANTY/DISCLAIMER: THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER THE CONSTRUCTED EXCEPT IN THE SPECIFIC CASES WHERE THE ENGINEER INSPECTS AND CONTROLS THE PHYSICAL CONSTRUCTION ON A CONTEMPORANEOUS BASIS AT THE SITE.
- AFETY NOTICE TO CONTRACTOR: IN ACCORDANCE WITH GENERALLY ACCEPTED ONSTRUCTION PRACTICES, THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE IOB SITE. INCLUDING SAFFTY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. ANY CONSTRUCTION OBSERVATION BY THE ENGINEER OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.
- ALL CONSTRUCTION IN STATE HIGHWAY DEPARTMENT RIGHT-OF-WAY SHALL BE COORDINATED WITH THE HIGHWAY DEPARTMENT RESIDENT ENGINEER.
- WETLANDS NOTE: ANY DEVELOPMENT, EXCAVATION, CONSTRUCTION, OR FILLING IN A U.S. CORPS OF ENGINEERS DESIGNATED WETLAND IS SUBJECT TO LOCAL, STATE AND FEDERAL APPROVALS, THE CONTRACTOR SHALL COMPLY WITH ALL PERMIT REQUIREMENTS AND/OR RESTRICTIONS AND ANY VIOLATION WILL BE SUBJECT TO FEDERAL PENALTY. THE CONTRACTOR SHALL HOLD THE OWNER/DEVELOPER, THE ENGINEER AND THE LOCAL GOVERNING AGENCIES HARMLESS AGAINST SUCH VIOLATION.
- RESIDENT ENGINEERING SERVICES: WHEN REQUESTED BY THE OWNER, RESIDENT NGINEERING SERVICES SHALL BE PROVIDED BY THE ENGINEERS (ON A TIME AND FREQUENCY BASIS) ACCEPTABLE TO THE CITY ENGINEER FOR IMPROVEMENTS TO PUBLIC WATER MAINS, PUBLIC SEWER, AND CITY STREETS, AT THE COMPLETION OF CONSTRUCTION, THE ENGINEER SHALL CERTIFY THE CONSTRUCTION TO BE IN COMPLIANCE WITH THE PLANS AND SPECIFICATIONS. THIS WORK WILL BE AT THE OWNER/DEVELOPER'S DIRECT EXPENSE AND SHALL BE COORDINATED WITH CELENGINEERING ASSOCIATES. INC. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE RESIDENT ENGINEER OF ANY PRECONSTRUCTION / CONSTRUCTION CONFERENCES AND ANY PUBLIC CONSTRUCTION 24

FLOOD CERTIFICATION:

BE LOCATED WITHIN ZONE 'X', OR AREAS OUTSIDE THE 0.2% ANNUAL CHANCE 100-YR FLOODPLAIN AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM. FLOOD INSURANCE MAP FOR BENTON COUNTY, ARKANSAS. MAP NUMBER: 05007C0260k MAP REVISED: JUNE 5TH, 20:

CITY OF ROGERS GENERAL NOTES:

- A. A PRE-CONSTRUCTION MEETING WITH THE ROGERS BUILDING DEPARTMENT IS REQUIRED PRIOR TO ISSUANCE OF A BUILDING PERMIT.
- B. A PRE-CONSTRUCTION MEETING WITH ROGERS PLANNING DEPARTMENT IS REQUIRED PRIOR TO ISSUANCE OF A GRADING PERMIT. ALL PHASE I EROSION CONTROL MEASURES MUST BE INSTALLED PRIOR TO THE MEETING AND A REDUCED SET OF APPROVED CONSTRUCTION PLANS (11X17) WILL BE REQUIRED
- EASEMENT PLAT MUST BE APPROVED AND RECORDED PRIOR TO CERTIFICATE OF OCCUPANCY.
- D. ALL CIVIL PLANS USED ON SITE MUST INCLUDE MOST CURRENT STAMP BY ROGERS PLANNING.
- E. IF PROJECT COSTS EXCEED \$20,000, A STATE CONTRACTOR'S LICENSE IS REQUIRED PER ROGERS CODE SEC 14-228(6).
- F. IT IS THE RESPONSIBILITY OF THE ENGINEER OF RECORD TO INCLUDE ALL ITEMS REQUIRED BY ORDINANCE. IF APPROVED PLANS CONFLICT WITH ORDINANCE REQUIREMENTS, THE ORDINANCE TAKES PRECEDENCE UNLESS A WAIVER OR VARIANCE HAS BEEN GRANTED
- G. STOCKPILES WITH EXCESS CONSTRUCTION SITE MATERIALS, INCLUDING BUT NOT LIMITED TO EXCESS TOPSOIL STOCKPILES, MUST BE REMOVED FROM SITE PRIOR TO PROJECT COMPLETION. GRADING BEYOND THE APPROVED LIMITS IS PROHIBITED.
- H. CHANNEL, DETENTION PONDS, RETENTION PONDS, SEDIMENT BASINS, ETC. REQUIRE SOD TO THE 100-YEAR WATER SURFACE ELEVATION WITHIN 2 WEEKS (14-DAYS) AFTER THE POND IS BUILT, AND THE POND MUST BE INSTALLED AT THE BEGINNING OF THE PROJECT WHERE
- WHERE PUBLIC STREETS END FOR FUTURE STUBS OUTS, INSTALL MUTCD CLASS 3 BARRICADE WITH "FUTURE STREET EXTENSION" SIGN CENTERED AT THE END OF THE RIGHT-OF-WAY.
- J. WHERE PUBLIC STREETS, TRAILS OR SIDEPATHS END, INCLUDE "SIDEWALK ENDS" SIGN.
- K. PUBLIC STREETS HAVE 20-FT OF DRIVABLE SURFACE FOR TWO WAY TRAFFIC. THIS DOES NOT
- . PLANTING THAT EXCEED THE MINIMUM REQUIREMENTS MAY NOT BE REDUCED ONCE APPROVED BY PLANNING COMMISSION.
- M. PROPOSE ROW DEDICATION AND DRAINAGE EASEMENT PLAT WILL BE SUBMITTED AFTER LARGE SCALE DEVELOPMENT PLAN SUBMITTAL APPROVAL.
- N. A SEPARATE SIGN PERMIT WILL BE REQUIRED FOR ANY SIGNAGE. APPROVAL OF THIS LSD DOES

LEGAL DESCRIPTION

A PART OF THE W1/2 OF THE SE1/4 OF SECTION 34, TOWNSHIP 20 NORTH, RANGE 30 WEST. BENTON COUNTY, ARKANSAS, AND BEING DESCRIBED AS FOLLOWS: COMMENCING AT THE SE COMER OF THE W 1/2 OF THE SE 1/4 OF SECTION 34. TOWNSHIP 20 NORTH, RANGE 30 WEST, SAID POINT BEING IN THE CENTERLINE OF ARKANSAS STATE HIGHWAY 102; THENCE ALONG THE CENTERLINE OF ARKANSAS STATE HIGHWAY 102 NORTH 89°55'25" WEST 442.06 FEET AND NORTH 00°25'34" WEST 45 00 FEET TO THE TRUE POINT OF BEGINNING, SAID POIN BEING A SET IRON PIN. ON THE NORTH RIGHT-OF-WAY OF ARKANSAS STATE HIGHWAY 102. THENCE ALONG SAID RIGHT-OF-WAY NORTH 89°55'27" WEST 55.80 FEET TO A SET IRON PIN THENCE LEAVING SAID RIGHT OF WAY NORTH 00°29'24" WEST 608.99 FEET TO A FOUND IRON PIN; THENCE NORTH 89°53'50" WEST 600.82 FEET TO A FOUND IRON PIN; THENCE NORTH 00°31'07" WEST 1320.04 FEET TO A SET IRON PIN, AS SHOWN IN PLAT RECORD BOOK 5 AT PAGE 81, THENCE NORTH 89°59'49" EAST 659.42 FEET TO A FOUND IRON PIN; THENCE SOUTH 00°25'34" EAST 1930.19 FEET TO THE TRUE POINT OF BEGINNING. SUBJECT TO THE RIGHT-OF-WAY OF ARKANSAS STATE HIGHWAY 102 ALONG THE SOUTH SIDE THERE OF.

LITTLE FLOCK TRACT 1 DESCRIPTION - P.N. 23-00278-070:

A PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER (SW/4 SE/4) OF SECTION 34, TOWNSHIP 20 NORTH, RANGE 30 WEST, CITY OF LITTLE FLOCK, BENTON COUNTY, ARKANSAS, MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE SE CORNER OF SAID SW/4 SE/4; THENCE N 87°02'34" W 190.54 FEET; THENCE N 02° 24'34" F 790.87 FFFT TO THE POINT OF BEGINNING: THENCE N 87°35'44" W A DISTANCE OF 255.02 FEET; THENCE N 02°14'16" E A DISTANCE OF 134.80 FEET; THENCE S 87°41'20" E A DISTANCE OF 255.05 FEET; THENCE S 02°15'02" W A DISTANCE OF 135.21 FEET TO THE POINT OF BEGINNING, WITH AN AREA OF 34431.51 SQUARE FEET, OR 0.790 ACRES, MORE OR LESS, AND SUBJECT TO ALL RIGHTS-OF-WAY, FASEMENTS AND RESTRICTIVE COVENANTS OF RECORD OF

LITTLE FLOCK TRACT 2 DESCRIPTION - P.N. 23-00278-065:

A PART OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER (SW/4 SE/4) OF SECTION 34, TOWNSHIP 20 NORTH, RANGE 30 WEST, CITY OF LITTLE FLOCK, BENTON COUNTY, ARKANSAS, MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE SE CORNER OF SAID SW/4 SE/4: THENCE N 87°02'34" W 190.54 FEET: THENCE N 02°24'34" E 790.87: THENCE N 02°15'02" E $135.21~{\sf FEET}$ TO THE POINT OF BEGINNING; THENCE N $87^{\circ}41^{\circ}20^{\circ}$ W A DISTANCE OF $255.05~{\sf FEET}$; THENCE N 02°14'16" E A DISTANCE OF 134.90 FEET; THENCE S 87°39'39" E A DISTANCE OF 255.08 FEET; THENCE S 02°15'02" W A DISTANCE OF 134.78 FEET TO THE POINT OF BEGINNING, WITH AN AREA OF 34392.33 SQUARE FEET, OR 0.790 ACRES, MORE OR LESS, AND SUBJECT TO ALL RIGHTS-OF-WAY, EASEMENTS AND RESTRICTIVE COVENANTS OF RECORD OR FACT.



PHONE:

PHONE:



VICINITY MAP

Latitude: 36°21'34"N

Longitude: 94°09'33"W

OWNER CONTACT:

ADDRESS: P.O. BOX 236 CAVE SPRINGS, AR 72718

CEI Engineering

NAME: WIL-SHAR, INC.

PHONE: (479) 986-0311

EMAIL: BILLY@WILSHAR.COM



PLANINDEX

	IN INDLA.		
C0.00	COVER SHEET	C5.10	EROSION CONTROL NOTES & DETAILS
C1.00	OVERALL DEMOLITION PLAN	C6.00	UTILITY PLAN
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DRAINAGE PLAN ENLARGEMENT

FROSION CONTROL PLAN PHASE 1 FNLARGEMENT **EROSION CONTROL PLAN PHASE 1 ENLARGEMENT 2** EROSION CONTROL PLAN PHASE 2 ENLARGEMENT 1

EROSION CONTROL PLAN PHASE 2 ENLARGEMENT 2

ASSOCIATED PLANS:

SURVEY

2. RWU WATER DETAILS

3. AUTOTURN EXHIBIT

RESOURCE LIST:

301 W. CHESTNUT ST., ROGERS, AR 72756 JOHN MCCURDY

EMAIL: JMCCURDY@ROGERSAR.GOV PH: (479) 621-1186

> ROGERS FIRE DEPARTMENT 201 N. 1ST ST... ROGERS, AR 72756 BILL RUMSEY, CAPTAIN EMAIL: WRUMSEY@ROGERSAR.GOV PH: (479) 621-1100

CITY OF ROGERS INSPECTION DEPARTMEN 113 NORTH 4TH ST., ROGERS, AR 72756 WILLIAM HYDE EMAIL: WHYDE@ROGERSAR.GOV PH: (479) 621-1100

3101 OAK STREET ROGERS, AR 72756 FRANKIE GUYII EMAIL:TRANSPORTATION@ROGERSAR.GOV PH: (479) 621-1140

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ROGERS WATER UTILITIES 601 S. 2ND ST. ROGERS, AR 72756 STEPHEN PONDER EMAIL:STEPHENPONDER@RWU.ORG PH: (479) 621-1142

COMMUNITY DEVELOPMENT 301 W. CHESTNUT ST., ROGERS, AR 72756 EMAIL:DCOBB@ROGERSAR.GOV PH: (479) 621-1186

4815 W. MARKHAM ST LITTLE ROCK, AR 72205 ADAM PARKER EMAIL:ADAM.PARKER@ARKANSAS.GOV PH: (501) 661-2824

PH: (479) 333-7004 CARROLL ELECTRIC 707 SE WALTON BLVD. BENTONVILLE, AR 72712-0329

130 FEDERAL WAY, PO BOX2129

EMAIL:BRANDON.STUBBS@BLACKHILLSCORP.CO

NATURAL GAS BLACK HILLS ENERGY

LOWELL, AR 71745

BRANDON STUBBS

DEREK THURMAN EMAIL:DTHURMAN@CARROLLECC.COM PH: (479) 273-2421

> 1133 E. HAROLD ST., FAYETTEVILLE, AR 72703 SCOTT SEAMAN EMAIL:SS7513@ATT.COM PH: (479) 442-1967

> > COX COMMUNICATIONS 4901 S. 48TH ST., SPRINGDALE, AR 72762 DOUG ELLISON PH: (479) 717-3608

CITY OF ROGERS STAMP

COVER SHEET

PROFESSIONAL OF RECORD

PROJECT MANAGER

CEI PROJECT NUMBER

32987 4/1/2024

DESIGNER

REVISION

SHEET NUMBER

CIVIL ENGINEERING • LANDSCAPE ARCHITECTURE • LAND SURVEYING • PLANNING BENTONVILLE | DALLAS | FRESNO | HOUSTON | JACKSONVILLE | MINNEAPOLIS | PHILADELPHIA | PHOENIX

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CITYVIEW PROJECT #PL202300328

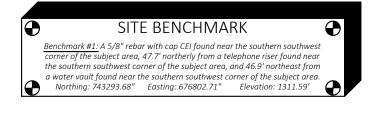
Land and Life CEI ENGINEERING ASSOCIATES, INC. 2600 NE 11TH ST BENTONVILLE, AR 72712 PHONE: (479) 273-9472 FAX: (479) 273-0844 REVISION NO. DESCRIPTION

REV-2

7/10/2023

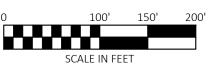
8/22/2023

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Know what's **below. Call** before you dig.

	 Boundary Line 	◆	Benchmark (BM)
	 Adjoining Boundary Line 	S	Sewer Manhole (SMH)
	Right-of-Way Line		Drainage Manhole (DMH
	Break Line	-6-	Fire Hydrant
OHE	Overhead Electric Line	$\overline{\mathbb{W}}$	Water Vault
Ę	— Centerline	M	Water Valve
	– Gas Line	6	Gas Meter
ooo	Chainlink Fence Line	Ĝ	Warning Gas Line Sign
xxx	Barb Wire Fence Line	£	Electric Meter
SS	 Sanitary Sewer Line 	\rightarrow	Guy Wire / Anchor
=====(Size)======	_ Storm Drainage Pipe	- ⇔	Light Pole Overhanging
W	– Water Line	-Ö°Ö-	Light Pole (2 Lamps)
	Found Monument (As Noted)	<i>p</i>	Utility Pole
•	Found Pipe (As noted)	T	Telephone Riser
♦	Set 5/8" Rebar "LS#1618"		Bollard / Guard Post
*	Found Railroad Spike		Sign (Type of Sign)
		R/W	Right-of-Way
		MSP	Master Street Plan

SIT	E DATA
SITE AREA (ROGERS)	20.71 ACRES (901,914± SF)
SITE AREA (LITTLE FLOCK)	1.57 ACRES (68,515± SF)
DEVELOPMENT AREA (ROGERS)	20.28 ACRES (883,581± SF)
DEVELOPMENT AREA (LITTLE FLOCK)	0.42 ACRES (18,333± SF)
BUILDING COVERAGE	0.00 ACRES (0± SF) : 0.00% (SA)
OPEN SPACE	6.72 ACRES (293,010± SF) : 30.6% (SA)
IMPERVIOUS AREA	15.25 ACRES (663,950± SF) : 69.4% (SA)
CURRENT ZONING	I1-CU
CURRENT USE	VEHICLE AND MATERIALS STORAGE
PROPOSED USE	VEHICLE AND MATERIALS STORAGE
ZONE I1	-CU SETBACK DATA
FRONT	50'
INTERIOR SIDE	25'
EXTERIOR SIDE	50'
REAR	25'
MAX. STRUCTURE HEIGHT	50'

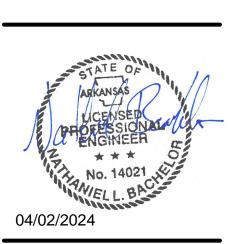
^{*}ADA PARKING TO BE PROVIDED IN FUTURE LARGE SCALE SUBMITTAL WHEN THE GRAVEL LOT IS PAVED.



CEI ENGINEERING ASSOCIATES, INC. 2600 NE 11TH ST BENTONVILLE, AR 72712 PHONE: (479) 273-9472 FAX: (479) 273-0844

REVISION				
NO.	DESCRIPTION	DATE		
1	REV-1	7/10/2023		
2	REV-2	8/22/2023		

300328 DROP YARD NWA
CITYVIEW PROJECT #PL2023
2609 WEST HUDSON ROAD
ROGERS, ARKANSAS

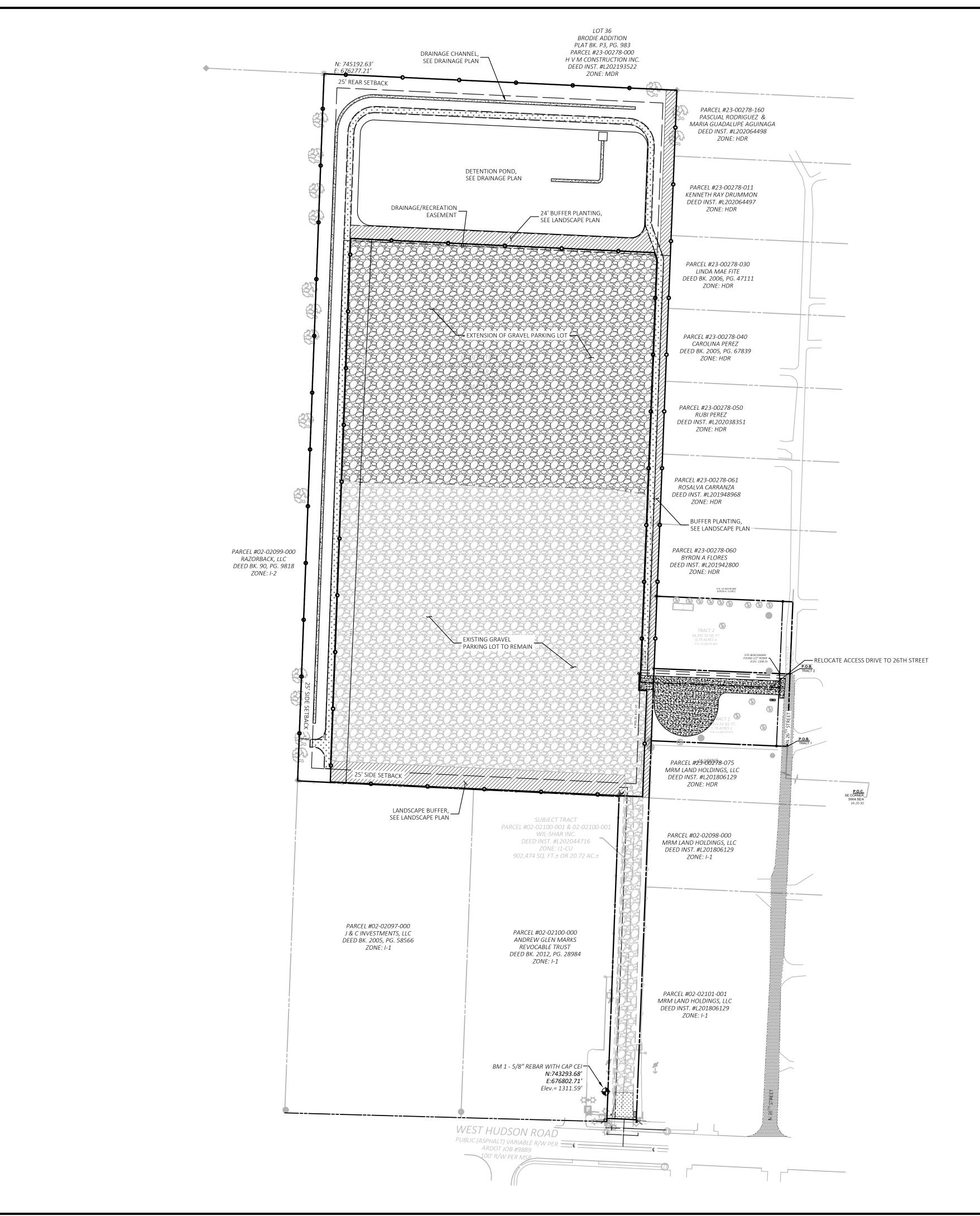


PROFESSIONAL OF RECORD	NLB
PROJECT MANAGER	CC
DESIGNER	TN
CEI PROJECT NUMBER	32987
DATE	4/1/2024
REVISION	REV-4

CITY OF ROGERS STAMP

OVERALL SITE PLAN

SHEET NUMBER



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Solutions for Land and Life

_
23
23

EXISTIN	G LEGEND			
		Boundary Line	•	Benchmark (BM)
		Adjoining Boundary Line	S	Sewer Manhole (SMH)
		Right-of-Way Line		Drainage Manhole (DMH)
		Break Line	-6-	Fire Hydrant
— ОНЕ		Overhead Electric Line		Water Vault
		Centerline	H	Water Valve
G -		Gas Line	9	Gas Meter
-0	·····	Chainlink Fence Line	G	Warning Gas Line Sign
_хх-	х	Barb Wire Fence Line	Ð	Electric Meter
SS-		Sanitary Sewer Line	\rightarrow	Guy Wire / Anchor
(Size)======	Storm Drainage Pipe	- -	Light Pole Overhanging
W		Water Line	-ذØ-	Light Pole (2 Lamps)
•		Found Monument (As Noted)	ø	Utility Pole
•		Found Pipe (As noted)	Ī	Telephone Riser
\Diamond		Set 5/8" Rebar "LS#1618"	•	Bollard / Guard Post
*		Found Railroad Spike	_	Sign (Type of Sign)
			R/W	Right-of-Way
			MSP	Master Street Plan

PROPOSED LEGEND TREE PROTECTION FENCE EXISTING TREE TO BE PRESERVED AND PROTECTED

LANDSCAPE NOTES

REFER TO SHEET L1.00 FOR PLANT SCHEDULE AND GENERAL NOTES REFER TO SHEET L2.00 FOR ADDITIONAL LANDSCAPE NOTES AND DETAILS

☐ LANDSCAPE DETAILS

50A TREE PLANTING DETAIL 50H TREE PROTECTION DETAIL

TREE MITIGATION CALCULATIONS

SIGNIFICANT TREES REMOVED PRIOR TO LSDP 8" DBH OR LARGER	143	5:1 REPLACEMENT = 715 REPLACEMENT TREES REQUIRED	
		ALTERNATIVE COMPLIANCE 2:5 REPLACEMENT = 58 REPLACEMENT TREES PROPOSED	TOTAL MITIGATION TREES PROVIDED = 72
SIGNIFICANT TREES REMOVED AS PART OF THIS LSDP 8" DBH OR LARGER	68	1:5 REPLACEMENT = 14 REPLACEMENT TREES REQUIRED	

LANDSCAPE REQUIREMENTS					
AREA	REQUIREMENT	REQUIRED	PROVIDED		
	20% OF THE SITE MUST BE GREEN SPACE	888,581 SF OF DEVELOPMENT AREA x 20% = 177,716 SF REQUIRED	180,155 SF PROVIDED		
LANDSCAPE AREA	ONE TREE OR SHRUB SHALL BE PROVIDED PER 1000 SF OF DEVELOPMENT AREA	888,581 SF OF DEVELOPED AREA / 1000 = 888 TREES OR SHRUBS REQUIRED	222 TREES AND 666 SHRUBS = TOTAL 888 TREES OR SHRUBS TO BE PROVIDED*		
	A MIN OF 25% OFF ALL REQUIRED PLANTINGS MUST BE TREES	888 x 25% = A MIN OF 222 TREES REQUIRED	222 TREES PROVIDED*		
	A MIN OF 10% OF ALL TREES MUST BE EVERGREEN	222 TREES PROVIDED x 10% = 23 EVERGREEN TREES REQUIRED	30 EVERGREEN TREES PROVIDED		

144 TREES PROVIDED IN THIS LSDP. WE ARE REQUESTING TO DIFFER 78 TREES TO A FUTURE LSDP.

GRAVEL AREA IS INTENDED TO BE PAVED ALONG WITH FUTURE BUILDING IMPROVEMENTS, SEE FUTURE IMPROVEMENTS EXHIBIT FOR EXTENTS OF FUTURE WORK AS PART OF A SEPARATE LSDP.

78 TREES ARE REQUESTED TO BE DEFERRED TO THE FUTURE CONSTRUCTION IN ORDER TO PREVENT DAMAGE TO TREES AND THE LANDSCAPE AREAS WHICH WILL BE DISTURBED DURING THE NEXT PHASE OF CONS

TREE MITIGATION
PLAN
SHEET TITLE

PROFESSIONAL OF RECORD

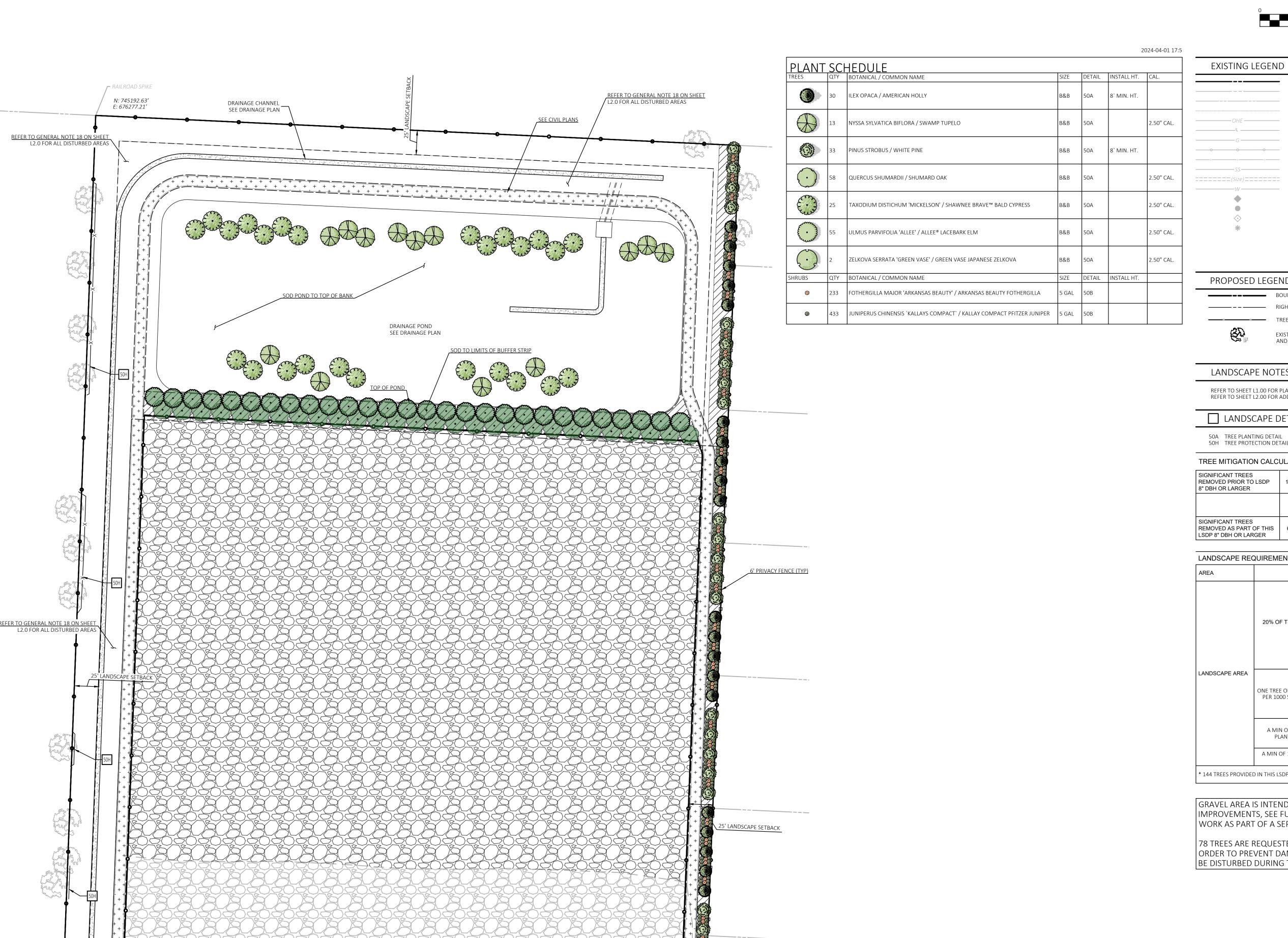
32987 4/2/2024 REV-4

PROJECT MANAGER

DESIGNER

SHEET NUMBER

STRUCTION.	CEI PROJECT NUMBER
	DATE
	REVISION
CITY OF ROGERS STAMP	
	TREE MITIGA PLAN SHEET TITLE
	SHEET NUMBER
CITY//IEW/ DBO/IECT #DI 202200220	

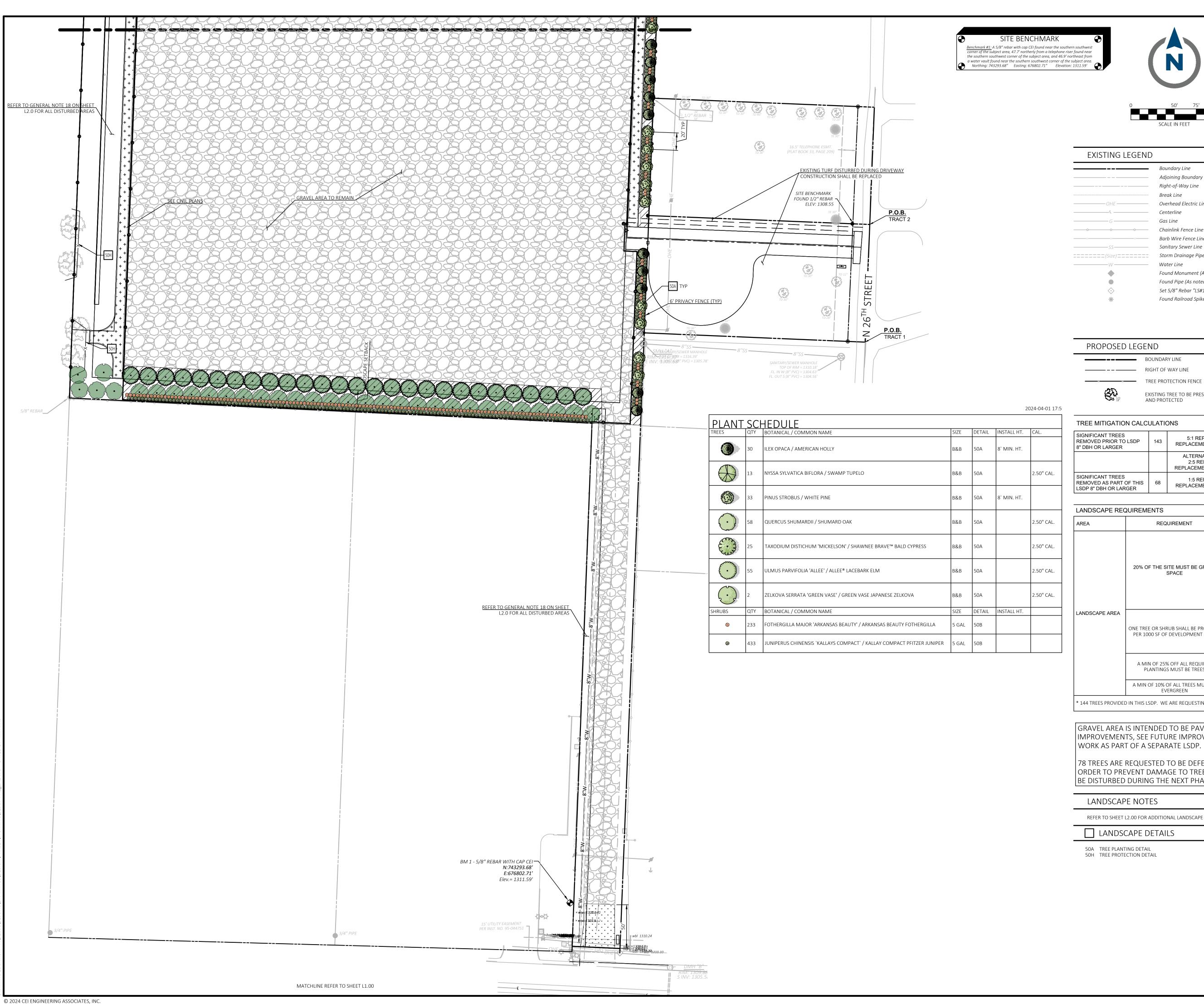


MATCHLINE REFER TO SHEET L1.00

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CITYVIEW PROJECT #PL202300328

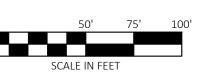
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Know what's below. Call before you dig.

	Boundary Line	◆	Benchmark (BM)
	—— Adjoining Boundary Line	(5)	Sewer Manhole (SMH)
	Right-of-Way Line		Drainage Manhole (DMH)
	Break Line	-6-	Fire Hydrant
—— ОНЕ ———	Overhead Electric Line		Water Vault
<u> </u>	—— Centerline	M	Water Valve
	—— Gas Line	6	Gas Meter
o	—— Chainlink Fence Line	Ĝ	Warning Gas Line Sign
xx	Barb Wire Fence Line	E	Electric Meter
SS	—— Sanitary Sewer Line	\rightarrow	Guy Wire / Anchor
==== (Size) =====	=== Storm Drainage Pipe	-	Light Pole Overhanging
	Water Line	-Ö°Ö-	Light Pole (2 Lamps)
•	Found Monument (As Noted)	,55	Utility Pole
•	Found Pipe (As noted)	T	Telephone Riser
\Diamond	Set 5/8" Rebar "LS#1618"	•	Bollard / Guard Post
*	Found Railroad Spike	_	Sign (Type of Sign)
		R/W	Right-of-Way
		MSP	Master Street Plan

TREE MITIGATION CALC	ULATIO	ONS	
SIGNIFICANT TREES REMOVED PRIOR TO LSDP 8" DBH OR LARGER	143	5:1 REPLACEMENT = 715 REPLACEMENT TREES REQUIRED	
		ALTERNATIVE COMPLIANCE 2:5 REPLACEMENT = 58 REPLACEMENT TREES PROPOSED	TOTAL MITIGATION TREES PROVIDED = 7
SIGNIFICANT TREES REMOVED AS PART OF THIS LSDP 8" DBH OR LARGER	68	1:5 REPLACEMENT = 14 REPLACEMENT TREES REQUIRED	

EXISTING TREE TO BE PRESERVED

AND PROTECTED

LANDSCAPE REQUIREMENTS

LANDSCAPE REC	ZUINEIVIEN I S		
AREA	REQUIREMENT	REQUIRED	PROVIDED
	20% OF THE SITE MUST BE GREEN SPACE	888,581 SF OF DEVELOPMENT AREA x 20% = 177,716 SF REQUIRED	180,155 SF PROVIDED
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	A MIN OF 10% OF ALL TREES MUST BE EVERGREEN	222 TREES PROVIDED x 10% = 23 EVERGREEN TREES REQUIRED	30 EVERGREEN TREES PROVIDED

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LANDSCAPE NOTES

REFER TO SHEET L2.00 FOR ADDITIONAL LANDSCAPE NOTES AND DETAILS

☐ LANDSCAPE DETAILS

50A TREE PLANTING DETAIL

CITY OF ROGERS STAMP

TREE MITIGATION PLAN 2

PROFESSIONAL OF RECORD

PROJECT MANAGER

CEI PROJECT NUMBER

32987

4/2/2024

DESIGNER

DATE

SHEET NUMBER

CITYVIEW PROJECT #PL202300328

Solutions for Land and Life CEI ENGINEERING ASSOCIATES, INC.

2600 NE 11TH ST BENTONVILLE, AR 72712 PHONE: (479) 273-9472 FAX: (479) 273-0844

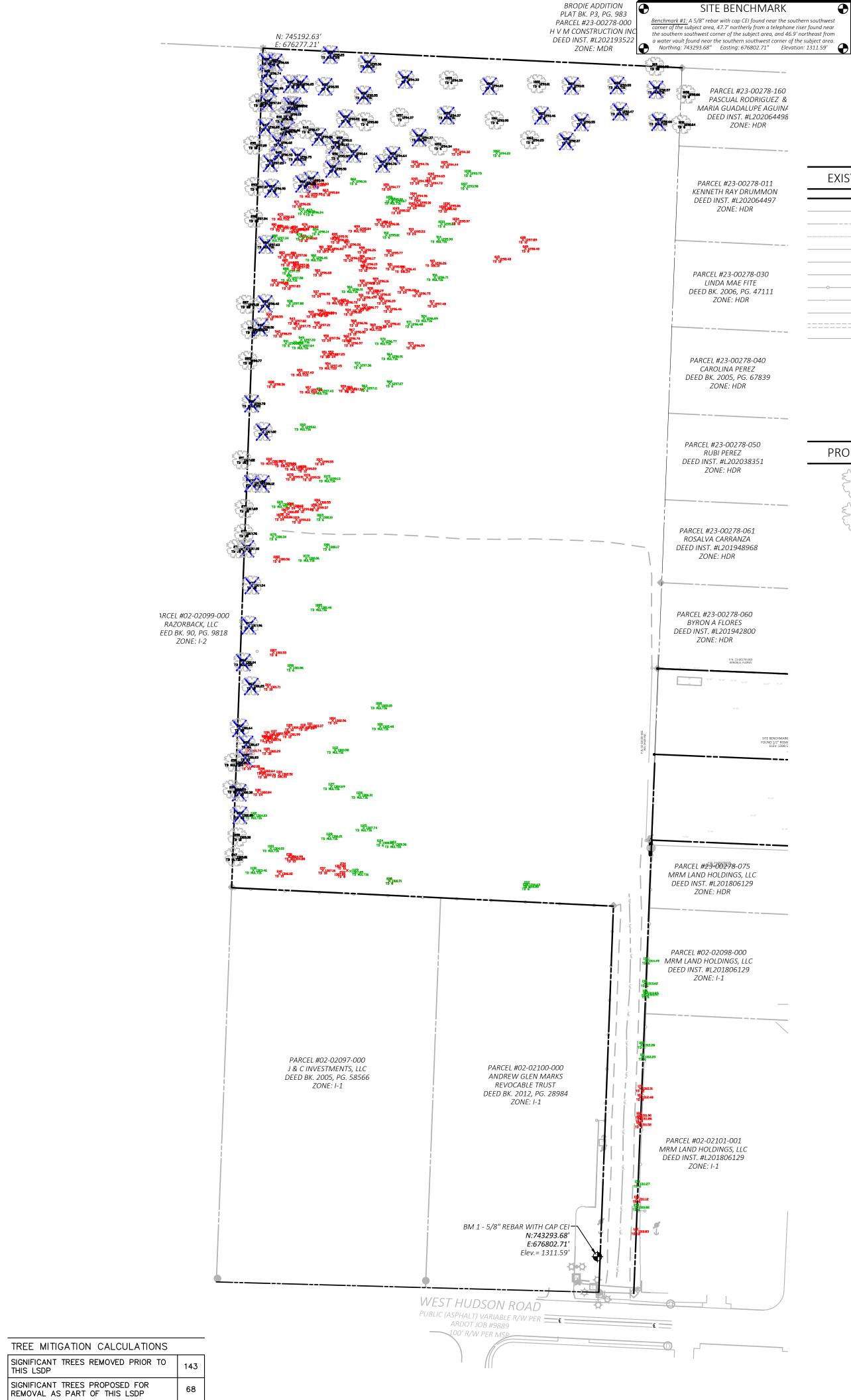
REVISION				
DESCRIPTION	DATE			
REV-1	7/10/2023			
REV-2	8/16/2023			
	DESCRIPTION REV-1			

300328

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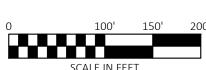
		TREE INVE	NTORY	
NO.	SPECIES	DBH	SIGNIFICANT TREE	CONDITION
142		12	X	REMOVED
153 154		6		REMOVED REMOVED
154		6		REMOVED
166		8	X	REMOVED
167		8	X	REMOVED
168		8	Х	REMOVED
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NO.	SPECIES	TREE INVE	SIGNIFICANT TREE	CONDITION
928 929		12 24	X	REMOVED REMOVED
930 931		24 12	X	REMOVED REMOVED
932 933		36 12	X X	REMOVED REMOVED
934		12	Х	REMOVED
935 936		15 6	X	REMOVED REMOVED
937 938		24	X	REMOVED REMOVED
939		12	X	REMOVED
940 941		30 6	X	REMOVED REMOVED
942 943		12 12	X X	REMOVED
944		12	Х	REMOVED REMOVED
945 946		24	X	REMOVED REMOVED
947 948		12 18	X X	REMOVED REMOVED
949		6	^	REMOVED
950 951		24	X	REMOVED REMOVED
952 953		24	X	REMOVED REMOVED
954		12	X	REMOVED
955 956		12 6	X	REMOVED REMOVED
957 959		12 36	X X	REMOVED REMOVED
960		36	X	REMOVED
962 963		6		REMOVED REMOVED
964		6		REMOVED
965 966		12	X	REMOVED REMOVED
967 968		15 10	X X	REMOVED REMOVED
969		12	X	REMOVED
970 971		6		REMOVED REMOVED
972		24	X	REMOVED
973 974		30 6	X	REMOVED REMOVED
1000 1001		6		REMOVED REMOVED
1004		24	X	REMOVED
1006 1007		24 24	X	REMOVED REMOVED
1008 1009		10 24	X X	REMOVED EXISTING
1010		6		REMOVED
1011 1012		36 10	X	EXISTING REMOVED
1013 1014		24 10	X	REMOVED REMOVED
1015		6		REMOVED
1016 1017		24	X	REMOVED REMOVED
1019 1022		12 6	X	REMOVED EXISTING
1023		8	X	EXISTING
1024 1025		12 12	X X	EXISTING EXISTING
1026 1027		6 10	X	EXISTING EXISTING
1028		6		EXISTING
1029 1030		10 8	X X	EXISTING EXISTING
1031 1032		6 12	X	EXISTING EXISTING
1033		6		REMOVED
1034 1035		10 12	X X	REMOVED REMOVED
1036		20	Х	REMOVED
1037 1038		6		REMOVED REMOVED
1039 1040		10 20	X	REMOVED REMOVED
1041 1042		10 24	X X	REMOVED
1043		24	Х	REMOVED REMOVED
1044 1045		24	X	REMOVED REMOVED
1046		20	Х	REMOVED
1047 1048		20	X	EXISTING EXISTING
1049 1050		24 6	X	REMOVED REMOVED
1051		24	Х	REMOVED
1052 1053		24 6	X	EXISTING EXISTING
1054 1055		24 6	Х	REMOVED EXISTING
1056		12	X	EXISTING
1057 1058		N/A 8	X	EXISTING EXISTING
1059 1060		6 10	X	EXISTING EXISTING
1061		6	^	EXISTING
1062 1063		6		REMOVED EXISTING
1066 1067		24	X	REMOVED REMOVED
1069	_	6	^	REMOVED
1070 1072		6		REMOVED REMOVED
1073 1074		12 12	X	REMOVED REMOVED
1075		6		REMOVED
1076 1077		12 12	X	REMOVED REMOVED
1078		20	Х	REMOVED
1079 1080		20 24	X	REMOVED EXISTING
1081 1082		6 8	X	REMOVED REMOVED
1085		6		REMOVED
1087 1090		8 6	X	REMOVED REMOVED
1093 1094		24 12	X X	REMOVED REMOVED
1095		24	Х	EXISTING
1096 1097		30	X X	REMOVED REMOVED
1098 1099		20 20	X X	REMOVED EXISTING
1100		24	X	REMOVED
1102 1103		6 6		REMOVED REMOVED
1104 1105		20 30	X X	REMOVED REMOVED
1106		20	Х	REMOVED
1107 1108		20 12	X X	REMOVED REMOVED
1109 1110		12 12	X	REMOVED REMOVED
1111		24	X	REMOVED REMOVED
		, , , , , , , , , , , , , , , , , , , ,		. NEIVIO V LD
1112 1123		6		REMOVED
1112				REMOVED REMOVED REMOVED REMOVED



LOT 36



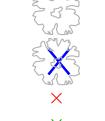




Know what's **below**. **Call** before you dig.

	SCALL IN FEET	Cai	I before you di
EXISTING LEGEND			
	Boundary Line	•	Benchmark (BM)
	Adjoining Boundary Line	<u>\$</u>	Sewer Manhole (SMH)
	Right-of-Way Line		Drainage Manhole (DMH,
	Break Line	-6-	Fire Hydrant
——— ОНЕ ———	Overhead Electric Line		Water Vault
<u> </u>	Centerline	M	Water Valve
	Gas Line	6	Gas Meter
	Chainlink Fence Line	Ĝ	Warning Gas Line Sign
xxx	Barb Wire Fence Line	£	Electric Meter
SS	Sanitary Sewer Line	\rightarrow	Guy Wire / Anchor
(Size)	Storm Drainage Pipe	-	Light Pole Overhanging
	Water Line	-Ö°Ö-	Light Pole (2 Lamps)
♦	Found Monument (As Noted)	<i>p</i>	Utility Pole
•	Found Pipe (As noted)	T	Telephone Riser
\Diamond	Set 5/8" Rebar "LS#1618"		Bollard / Guard Post
*	Found Railroad Spike	-	Sign (Type of Sign)
		R/W	Right-of-Way
		MSP	Master Street Plan

PROPOSED LEGEND



REMAINING EXISTING TREES

SIGNIFICANT TREE TO BE REMOVED

REMOVED SIGNIFICANT TREES 8" DBH OR LARGER
REMOVED TREES 6" DBH OR SMALLER

DROP YARD NWA
CITYVIEW PROJECT #PL202300328
2609 WEST HUDSON ROAD
ROGERS, ARKANSAS

Solutions for Land and Life

CEI ENGINEERING ASSOCIATES, INC.

2600 NE 11TH ST BENTONVILLE, AR 72712 PHONE: (479) 273-9472 FAX: (479) 273-0844



NLB
CC
TN
32987
2/2024
REV-4

TREE SURVEY

CITY OF ROGERS STAMP

SHEET NUMBER

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