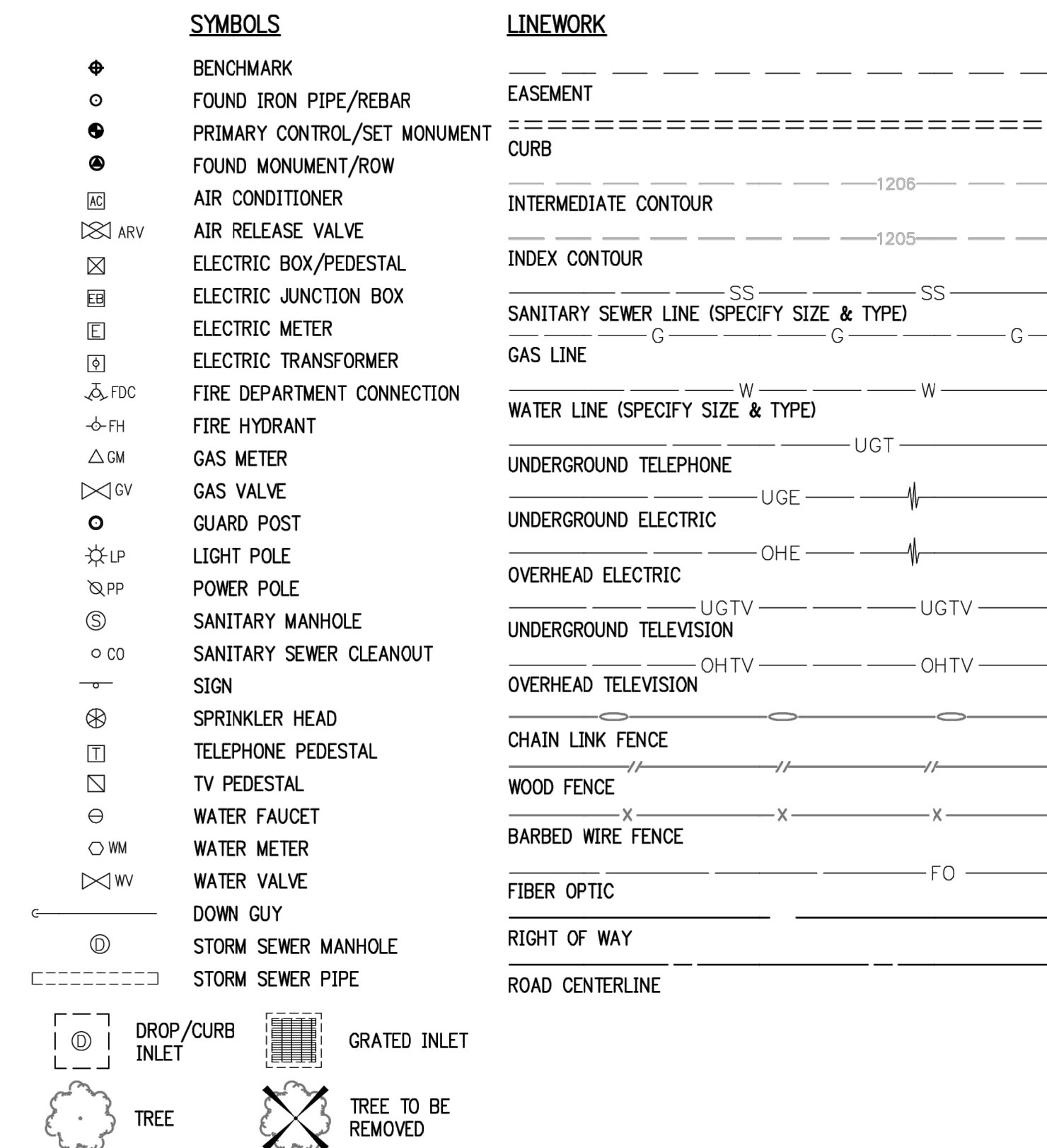
[illegible]

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C-002



ONLY THE CONTROL POINTS, COORDINATE VALUES, AND ELEVATIONS SHOWN ON THIS SHEET SHALL BE USED ON THIS PROJECT. CRAFTON, TULL & ASSOCIATES SHALL NOT BE RESPONSIBLE FOR ANY CONSEQUENCES OF USING CONTROL POINTS, COORDINATE VALUES OR ELEVATIONS ESTABLISHED OR DERIVED FROM OTHER SOURCES. CRAFTON, TULL & ASSOCIATES, AT ITS SOLE DISCRETION, MAY ALLOW THE USE OF ALTERNATIVE OR ADDITIONAL CONTROL, BY A WRITTEN AMENDMENT TO THIS SHEET SEALED BY THE RESPONSIBLE SUPERVISOR. USERS OF THIS SHEET SHALL BE RESPONSIBLE FOR CAREFULLY CHECKING THE FIELD PRACTICE AND IMMEDIATELY NOTIFY THE CRAFTON, TULL & ASSOCIATES PROJECT ENGINEER OF ANY INCONSISTENCIES IN THE OBSERVED COORDINATE VALUES, ELEVATIONS AND DESCRIPTIONS FOR CONTROL POINTS SHOWN ON THIS SHEET.

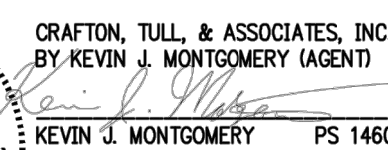
CONTROL POINT TABLE				
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
127	742580.80	686878.95	1334.45	5/8"RBR
129	742105.20	686546.79	1336.24	5/8"RBR

THE ABOVE CONTROL POINT TABLE ITEM VALUES DO NOT CONSTITUTE A "SURVEYING MEASUREMENT CERTIFICATION" AS DEFINED BY ARKANSAS CODE SECTION 17-48-101 AS OF JULY 2014. THERE IS NO WARRANTY TO THIRD PARTIES AS TO ACCURACY OR CORRECTNESS OF THE NUMERICAL VALUES SHOWN HEREON.

BASIS OF BEARING: ARKANSAS STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NAD 83  
 HORIZONTAL DATUM AND NAVD 88 HORIZONTAL DATUM DETERMINED BY GPS OBSERVATION ON THE  
 CITY OF ROGERS CONTROL MONUMENTS USING THEIR PUBLISHED VALUES.  
 BEARINGS: GRID  
 DISTANCE: GRID  
 CONVERGENCE: ANGLE: -01°14'09.50"  
 COMBINED SCALE FACTOR: 0.99996246  
 VALUE CALCULATED AT THE FOLLOWING POINT:  
 N 742580.80 E 686878.95 Z 1334.45

PROJECT CONTROL CERTIFICATE:

I HEREBY DECLARE THAT ON THE 9TH DAY OF FEBRUARY, 2024 THE ABOVE CONTROL POINT TABLE ITEM VALUES WERE CHECKED TO FOR CONSISTENCY WITH THE FIELD DATA FOR EACH IT PROVIDED BY THE FIELD CREW UNDER MY SUPERVISION. THE CONTROL HORIZONTAL AND VERTICAL DATUM WAS CONFIRMED IN RELATION TO THE RELEVANT LOCAL MUNICIPAL CONTROL SYSTEM. THIS CERTIFICATION IS APPLICABLE ONLY TO THE CONTROL POINT VALUES AS THEY EXISTED AT THE TIME OF IMPLEMENTATION BY THE FIELD CREW UNDER MY SUPERVISION. THIS CERTIFICATE DOES NOT EXTEND TO ANY ITEM OR DEPICTION SHOWN HEREON, OTHER THAN THE CONTROL POINT VALUES SHOWN IN THE CONTROL POINT TABLE.



NOTE:  
THIS PLAN SHEET DOES NOT CONSTITUTE A BOUNDARY SURVEY OR ALTA/ASPS LAND TITLE  
SURVEY. EXCEPT FOR THE CONTROL POINT TABLE VALUES, ALL ELSE IS SHOWN FOR  
INFORMATION ONLY.













C-102





**LEGEND (CONSTRUCT)**

- SYMBOLS**
  - MANHOLE
- LINWORK**
  - EASEMENT
  - ASPHALT
  - SANITARY SEWER LINE
  - RIGHT OF WAY
  - PROPERTY LINE

C-103









GRAPHIC SCALE IN FEET

A horizontal number line with tick marks at 0', 10', and 30'. The segment between 0' and 10' is shaded black.

W. HUDSON RD,  
ROGERS, AR

### Key Plan

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PROJECT NO: 23111000

ISSUE DATE: 04/05/2024

QC by: \_\_\_\_\_

## PRELIMINARY PLANS

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GRADING PLAN A

C-105

## SYMBOLS

FOUND IRON PIPE/REBAR  
ELECTRIC BOX/PEDESTAL  
ELECTRIC JUNCTION BOX  
ELECTRIC METER  
ELECTRIC TRANSFORMER  
LIGHT POLE  
POWER POLE  
SANITARY MANHOLE  
TELEPHONE PEDESTAL  
TV PEDESTAL

## LINEWORK

The diagram illustrates a road cross-section with the following components and offsets from the centerline:

- EASEMENT**: Indicated by a dashed line at the top, with an offset of 1206.
- CURB**: Indicated by a dashed line below the easement, with an offset of 1205.
- INTERMEDIATE CONTOUR**: Indicated by a solid line below the curb, with an offset of 1205.
- INDEX CONTOUR**: Indicated by a solid line below the intermediate contour, with an offset of 1205.
- RIGHT OF WAY**: Indicated by a solid line below the index contour.
- ROAD CENTERLINE**: Indicated by a solid line at the bottom.

## SYMBOLS

TC XX.XX TOP OF CURB  
TP XX.XX TOP OF PAVEMENT

## LINEWORK

CURB	
INTERMEDIATE CONTOUR	1206
INDEX CONTOUR	1205
PROPERTY LINE	

- THE DESIGN, INSPECTION, AND CERTIFICATION OF ANY RETAINING WALL SHOWN OR REFERENCED HEREIN, INCLUDING BUT NOT LIMITED TO, SEGMENTAL RETAINING WALLS, MASS GRAVITY WALLS, GABION WALLS, ETC., GREATER THAN FORTY-EIGHT INCHES IN HEIGHT, SHALL BE BY OTHERS. ANY RETAINING WALL DATA SHOWN OR REFERENCED HEREIN SHALL BE FOR COORDINATION OF THE WALL LOCATION AND ELEVATIONS ONLY.
2. THE OWNER/CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR OBTAINING AND PROVIDING SEPARATE AND INDEPENDENT RETAINING WALL DESIGNS, INSPECTIONS, AND CERTIFICATIONS BY A REGISTERED PROFESSIONAL ENGINEER OTHER THAN CRAFTON TULL. THIS SHALL ALSO APPLY TO ANY ASSOCIATED AND NECESSARY PUBLIC SAFETY DEVICES INCLUDING, BUT NOT LIMITED TO, PEDESTRIAN SAFETY RAILS.
3. THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF EXISTING UTILITIES ON SITE OR IN RIGHT-OF-WAY. ALL UTILITIES MUST BE LOCATED PRIOR TO GRADING START.
4. ALL WORK SHALL BE DONE IN STRICT ACCORDANCE WITH THE PROJECT SPECIFICATIONS.
5. ALL CUT OR FILL SLOPES SHALL BE A MAX 3:1 SLOPE OR FLATTER UNLESS OTHERWISE NOTED.
6. IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITION OR BETTER.
7. ALL STORM SEWER PIPE CONNECTIONS TO STRUCTURES SHALL BE GROUTED TO ENSURE CONNECTION AT STRUCTURE IS WATERTIGHT. ALL STORM SEWER STRUCTURES SHALL HAVE A SMOOTH UNIFORM POURED MORTAR INVERT FROM INVERT IN TO INVERT OUT.
8. ALL DRAINAGE STRUCTURES AND STORM SEWER PIPES SHALL MEET HEAVY DUTY TRAFFIC (H20) LOADING AND BE INSTALLED ACCORDINGLY WHEN IN PAVED AND TRAFFIC AREAS.
9. ALL STORM SEWER MANHOLES IN PAVED AREAS SHALL BE FLUSH WITH THE PAVEMENT AND SHALL HAVE TRAFFIC BEARING RINGS AND COVERS. MANHOLES IN UNPAVED AREAS SHALL BE 1" ABOVE FINISH GRADE. LIDS SHALL BE LABELED PER JURISDICTIONAL SPECIFICATIONS.
10. SITE GRADING SHALL NOT PROCEED UNTIL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. THE CONTRACTOR SHALL ADHERE TO ALL TERMS AND CONDITIONS AS OUTLINED IN THE GENERAL NOTES PERMIT AND THE SWPPP FOR STORMWATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
11. ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL TO FINAL GRADE. REFER TO THE LANDSCAPE PLAN.
12. TOPOGRAPHIC INFORMATION TAKEN FROM A TOPOGRAPHIC SURVEY BY LAND SURVEYORS. IF CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON PLANS, CONTACT ENGINEER IMMEDIATELY.
13. THE CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS THROUGHOUT ALL PHASES OF CONSTRUCTION.
14. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESICULES, SIDEWALKS, EXIST PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXIST UTILITY ENTRANCE LOCATIONS.
15. THE EARTHWORK FOR ALL BUILDING FOUNDATIONS AND SLABS SHALL BE IN ACCORDANCE WITH ARCHITECTURAL BUILDING PLANS AND SPECIFICATIONS.
16. EXISTING DRAINAGE STRUCTURES TO BE INSPECTED AND REPAIRED AS NEEDED, AND EXISTING PIPES TO BE CLEANED OUT TO REMOVE ALL SILT AND DEBRIS.
17. CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ENSURE A SMOOTH FIT AND CONTINUOUS GRADE.
18. CONTRACTOR SHALL MAINTAIN ALL EXISTING PARKING, SIDEWALKS, DRIVES, ETC. CLEAR AND FREE FROM ANY CONSTRUCTION ACTIVITY AND/OR MATERIAL TO ENSURE EASY AND SAFE PEDESTRIAN AND VEHICULAR TRAFFIC TO AND FROM THE SITE.
19. IF NET AREA IS ADJUDICATED ON-SITE THE CONTRACTOR SHALL COORDINATE WITH THE GEOTECHNICAL ENGINEER FOR THE DESIGN AND PLACEMENT OF A FRENCH DRAIN SYSTEM.
20. CRAFTON TULL & ASSOCIATES, INC. (CTA) HAS NOT NECESSARILY ESTABLISHED MINIMUM FINISH FLOOR ELEVATIONS FOR EACH INDIVIDUAL BUILDING OR LOT IN THIS SUBDIVISION. WHEN A MINIMUM BUILDING FLOOR ELEVATION IS NOT ESTABLISHED AND NOTED, THE ULTIMATE RESPONSIBILITY FOR THE PROPER GRADING OF EACH INDIVIDUAL LOT OR PARCEL SHALL REST WITH THE LOT'S OWNER. TYPICALLY, THE MINIMUM FINISH FIRST FLOOR ELEVATIONS SHOULD BE AT LEAST TWELVE INCHES (12") ABOVE THE FINISHED TOP OF STREET CURB ELEVATION ASSOCIATED WITH EACH INDIVIDUAL LOT OR PARCEL AS REQUIRED BY LOCAL AND STATE CODES.

CRAFTON TULL CANNOT CERTIFY THE SITE AS COMPLETE IN ORDER TO OBTAIN THE CERTIFICATE OF OCCUPANCY UNTIL ALL DISTURBED AREAS RELATED TO THE CONSTRUCTION OF THE PROJECT, BOTH ONSITE AND OFFSITE, HAVE BEEN STABILIZED PER THE PLANS AND SPECIFICATIONS AND ALL REQUIREMENTS SPELLED OUT IN PERMITS ISSUED BY THE STATE AND LOCAL AUTHORITIES HAVE BEEN MET.

DRAWINGS: G:\23111000\_GATEWAY\PL\INFRASTRUCTURE\CIVIL\DWG\ICRADING PLAN (EXHIBIT B).DWG  
LAYOUT: GRADING PLAN [B] , LAST SAVED: AES162, 4/5/2024 10:27:14 AM  
LAST PLOTTED BY: ALI KARR, 4/5/2024 10:32:57 AM



















