(LEGEND (EXISTING SYMBOLS)

	<u>SYMBOLS</u>	LINEWORK
\$	BENCHMARK	
O	FOUND IRON PIPE/REBAR	EASEMENT
•	PRIMARY CONTROL/SET MONUMENT	
۲	FOUND MONUMENT/ROW	CURB
AC	AIR CONDITIONER	INTERMEDIATE CON
I ARV	AIR RELEASE VALVE	
\boxtimes	ELECTRIC BOX/PEDESTAL	INDEX CONTOUR
EB	ELECTRIC JUNCTION BOX	
E	ELECTRIC METER	SANITARY SEWER L
¢	ELECTRIC TRANSFORMER	GAS LINE
, J, FDC	FIRE DEPARTMENT CONNECTION	WATER LINE (SPEC
-&- FH	FIRE HYDRANT	WATER LINE (SPECI
\bigtriangleup GM	GAS METER	UNDERGROUND TEL
GV	GAS VALVE	
0	GUARD POST	UNDERGROUND ELE
÷;¢ lp	LIGHT POLE	OVERHEAD ELECTRI
\& PP	POWER POLE	
S	SANITARY MANHOLE	UNDERGROUND TEL
0 C0	SANITARY SEWER CLEANOUT	
	SIGN	OVERHEAD TELEVIS
\otimes	SPRINKLER HEAD	CHAIN LINK FENCE
T	TELEPHONE PEDESTAL	
	TV PEDESTAL	WOOD FENCE
\ominus	WATER FAUCET	BARBED WIRE FENC
>WM	WATER METER	
VV [V]	WATER VALVE	FIBER OPTIC
	DOWN GUY	
\square	STORM SEWER MANHOLE	RIGHT OF WAY
	STORM SEWER PIPE	ROAD CENTERLINE

GRATED INLET

TREE TO BE REMOVED

NTOUR - ____ SS _____ SS _____ LINE (SPECIFY SIZE & TYPE) — — G — — G — G — ____ W _____ _ ECIFY SIZE & TYPE) —— UGT—— —— ELEPHONE LECTRIC _____ OHF _____ ----- UGTV ------ UGTV ------ELEVISION ----- OHTV ------ OHTV ------/ISION — — — FO — — FO — — FO — —

DROP/CURB

INLET

TREE

LINEWORK

EGEND (CONSTRUCT

	SYMBOLS	
•	SET IRON PIN	
*	LIGHT POLE	
X	POWER POLE	
	TELEPHONE PEDESTAL	
	TV PEDESTAL	
\odot	MANHOLE	
o	SANITARY SEWER CLEANOUT	
	GAS METER	
\mathbf{M}	GAS VALVE	
	STORM SEWER PIPE	
<u>(X-X)</u>	STRUCTURE NUMBER	
	WATER VALVE	
-	FIRE HYDRANT ASSEMBLY	
	AIR RELEASE VALVE	
*	FIRE DEPARTMENT CONNECTION	
•	WATER METER	
	BACK FLOW PREVENTER	
	REDUCER	
	RECTANGULAR DROP INLET, GRATED INLET OR JUNCTION BOX (SPECIFY ON PLAN SHEET)	
	CIRCULAR DROP INLET, GRATED INLET OR JUNCTION BOX (SPECIFY ON PLAN SHEET)	

EASEMENT		 	
CURB		 	
INTERMEDIATE CONTOUR			
INDEX CONTOUR			
SANITARY SEWER LINE			
GAS LINE			
WATER LINE			
UNDERGROUND TELEPHONE			
UNDERGROUND ELECTRIC			
OVERHEAD ELECTRIC			
FIBER OPTIC			
UNDERGROUND TELEVISION		O	
CHAIN LINK FENCE			
WOOD FENCE			
BARBED WIRE FENCE	X	X	
BUILDING SET BACK			
RIGHT OF WAY			
PROPERTY LINE		 	
ROAD CENTERLINE			

PROJECT INFOR	RMATION
BUILDING SETBACK:	FRONT 65' INTERIOR SIDE 15' EXTERIOR SIDE 45' REAR 15'
SITE AREA:	8.11 AC. (393,454
ZONING:	C-2 COMMERCIAL
PROPERTY USAGE: PROPOSED LOT COUNT:	3
MAXIMUM BUILDING HEIGHT:	-
ADDRESS:	W. HUDSON RD, ROGERS, AR
SITE OWNER:	GATEWAY PLAZA LIC 1400 W MARKHAM, LITTLE ROCK, AR 72 501–944–5858
SITE ENGINEER:	CRAFTON TULL A.KARR P.E. 901 N. 47TH ST., S ROGERS, AR 72756 479–636–4838

GENERAL NOTES

- LOCATION OF ALL FACILITIES BEFORE BEGINNING WORK.
- TULL & ASSOCIATES, INC.

- LINE.

SITE STABILIZATION AND CERTIFICATION

CRAFTON TULL CANNOT CERTIFY THE SITE AS COMPLETE IN ORDER TO OBTAIN THE CERTIFICATE OF OCCUPANCY UNTIL ALL DISTURBED AREAS RELATED TO THE CONSTRUCTION OF THE PROJECT, BOTH ONSITE AND OFFSITE, HAVE BEEN STABILIZED PER THE PLANS AND SPECIFICATIONS AND ALL REQUIREMENTS SPELLED OUT IN PERMITS ISSUED BY THE STATE AND LOCAL AUTHORITIES HAVE BEEN MET.

Arkansas One Call



GATEWAY PLAZA W. HUDSON RD, ROGERS, ARKANSAS PRELIMINARY PLAT PL2024XXXX







RY BUILDINGS

IG, LLC SUITE 204 72201

SUITE 400

THE DESIGN, INSPECTION, AND CERTIFICATION OF ANY RETAINING WALL SHOWN OR REFERENCED HEREIN, INCLUDING BUT NOT LIMITED TO, SEGMENTAL RETAINING WALLS, MASS GRAVITY WALLS, GABION WALLS, ETC., GREATER THAN FORTY-EIGHT INCHES IN HEIGHT, SHALL BE BY OTHERS. ANY RETAINING WALL DATA SHOWN OR REFERENCED HEREIN SHALL BE ONLY FOR COORDINATION OF THE WALL LOCATION AND ELEVATIONS. 2. THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES OR STRUCTURES SHOWN ON THESE DRAWINGS IS BASED ON A SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO EXISTING UTILITIES EXCEPT AS SHOWN ON THESE DRAWINGS. CRAFTON TULL

ASSUMES NO RESPONSIBILITY REGARDING THE ACCURACY OF THE DEPICTED LOCATION(S) OF THE UNDERGROUND FACILITIES ON THESE DRAWINGS. CONTRACTOR IS RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ANY OTHER FACILITIES NOT SHOWN ON THESE DRAWINGS. CONTRACTOR SHALL VERIFY 3. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS

OF THE JOB SITE INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN OR NEAR THE CONSTRUCTION SITE. 4. THIS DOCUMENT AND THE IDEAS AND DESIGNS INCORPORATED HEREIN, AS AN INSTRUMENT OF PROFESSIONAL SERVICES, IS THE PROPERTY OF CRAFTON, TULL & ASSOCIATES, INC. AND IS NOT TO BE USED, IN WHOLE OR PART, FOR ANY OTHER PROJECT WITHOUT THE WRITTEN AUTHORIZATION OF CRAFTON,

5. CONTRACTOR SHALL NOT CAUSE ANY LONG-TERM INCONVENIENCE TO THE PUBLIC, ADJACENT PROPERTY OWNERS, PEDESTRIANS, ETC. DURING CONSTRUCTION OF THIS PROJECT. CONTRACTOR SHALL PROVIDE ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES DURING CONSTRUCTION. 6. CONTRACTOR SHALL CONTACT THE ENGINEER FOR CLARIFICATION IF A DISCREPANCY OR INCONSISTENCY IS IDENTIFIED IN THE PLANS OR SPECIFICATIONS. CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND IMPLEMENTATION OF ALL SHEETING, SHORING, AND SPECIAL EXCAVATION MEASURES REQUIRED ON THE PROJECT WHICH ARE NECESSARY TO CONFORM TO OSHA, FEDERAL, STATE AND LOCAL REGULATIONS.

8. ENGINEER OF RECORD SHALL OBSERVE THE INSTALLATION OF THE WATER & SEWER MAINS. BEYOND THAT SCOPE, CRAFTON TULL & ASSOCIATES, INC. (CTA) HAS NOT BEEN RETAINED BY THE OWNER FOR CONSTRUCTION ADMINISTRATION OR OBSERVATION SERVICES FOR THE WORK INDICATED ON THESE DRAWINGS. THEREFORE, CTA HEREBY NOTIFIES ALL PARTIES INVOLVED THAT CRAFTON, TULL & ASSOCIATES, INC. ASSUMES NO RESPONSIBILITY FOR THE INTERPRETATION, COORDINATION, OR ADMINISTRATION OF THESE DOCUMENTS AND/OR DEVIATIONS THEREOF. FURTHERMORE, CRAFTON, TULL & ASSOCIATES, INC. WILL NOT BE RESPONSIBLE FOR ANY EFFECTS THAT ANY CHANGES TO THESE DOCUMENTS MAY HAVE ON ANY RELATED TRADES, CONSTRUCTION SEQUENCES, OR OPERATION OF THE COMPLETED PROJECT EXCEPT AS SPECIFICALLY NOTED IN THE AGREEMENT BETWEEN CRAFTON, TULL & ASSOCIATES AND THE OWNER. ENERGIZED ELECTRICAL LINE SAFETY, WARNINGS, AND ADVANCED NOTICES: ALL OWNERS, GENERAL CONTRACTORS, AND SUBCONTRACTORS ASSOCIATED WITH THIS PROJECT SHALL BE RESPONSIBLE FOR FAMILIARIZING THEMSELVES WITH, COMPLYING WITH, AND THE ENFORCEMENT OF ARKANSAS CODES AR ST § 11-5-307 AND § AR ST 11-5-308 AND ANY OTHER CURRENT STATE CODES PERTAINING TO ADVANCE NOTICE REQUIREMENTS AND FOR SAFETY OF ALL PERSONNEL, INCLUDING THE GENERAL PUBLIC, PERTAINING TO ANY WORK, MOVEMENT, AND ACTIVITY IN CLOSE PROXIMITY TO ANY ENERGIZED ELECTRICAL



WATER & SANITARY SEWER ROGERS WATER UTILITIES 601 S. 2ND ST. ROGERS, AR 72756 PHONE: (479)621–1142



THIS PROPERTY IS IN FLOOD ZONE "A" OR "AE", AND IS INSIDE THE 100-YEAR PLAIN AS SHOWN ON THE NATIONAL FLOOD INSURANCE PROGRAM'S FIRM PANEL NUMBER 05007C0280K, EFFECTIVE DATE 06/05/2012.

THE ABOVE STATEMENT IS FOR INFORMATION ONLY AND THIS SURVEYOR ASSUM LIABILITY FOR THE CORRECTNESS OF THE CITED MAP(S). IN ADDITION, NO INDEP STUDY OF THE LIKELIHOOD OF FLOODING OF THE SURVEYED PROPERTY HAS BEE THE SURVEYOR AND NO OPINION OF FLOOD HAZARDS IS INCLUDED IN THIS SUR FLOOD HAZARD ZONE BOUNDARIES SHOWN HEREON WERE SCALED FROM THE MA AND ARE NOT THE RESULT OF INDEPENDENT COMPUTATION OR ANALYSIS.

PROPERTY DESCRIPTION

TRACT 3A1 AND 3B1 AS PER PLAT RECORD BOOK 2016 PAGE 279 IN THE OFF THE CIRCUIT CLERK AND EX-OFFICIO RECORDER OF BENTON COUNTY TRACT 3A1

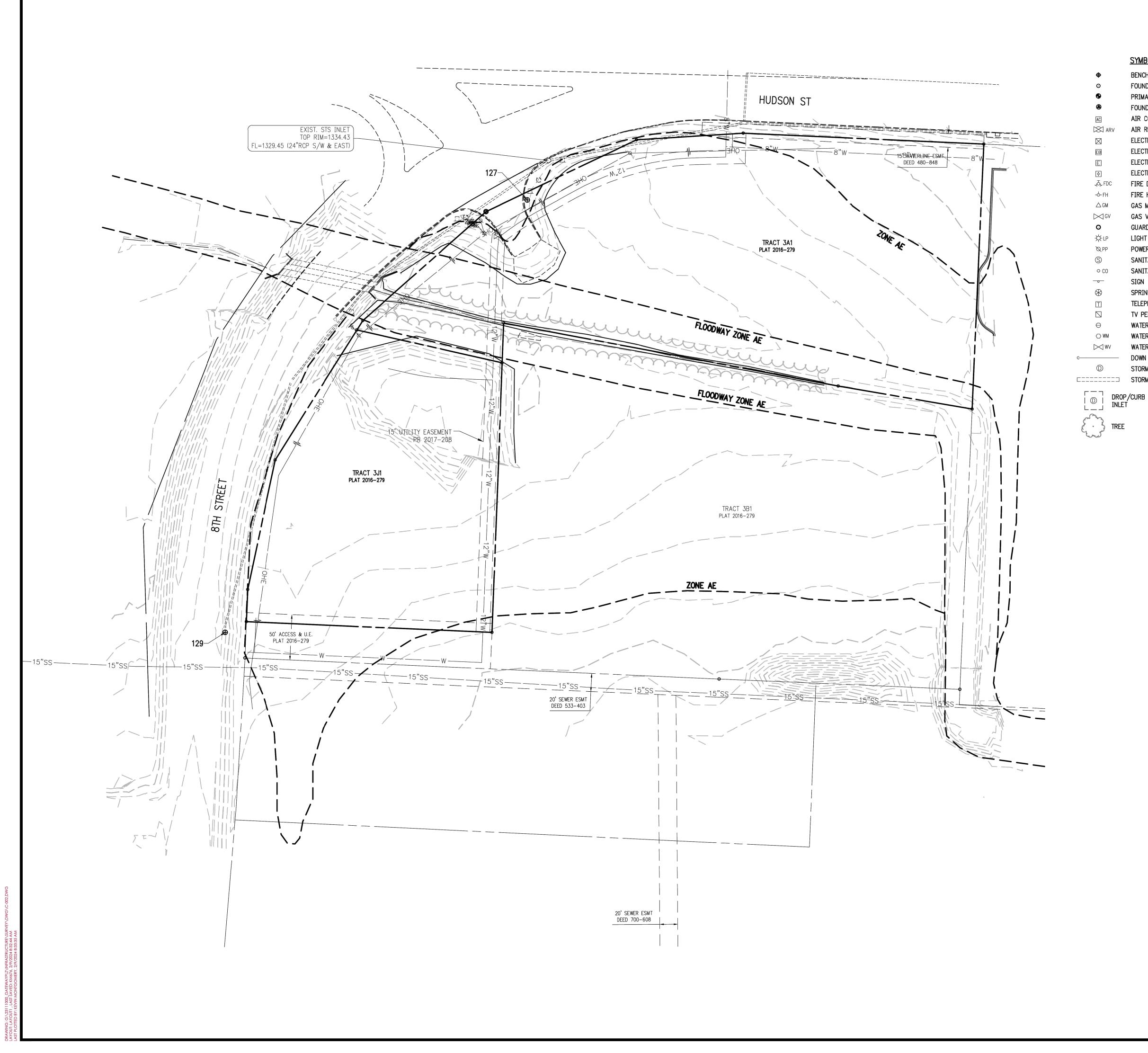
PART OF LOT 3 OF THE BOUNDARY SURVEY - LINDSEY, SEXTON, BALL AND MC PAGE 271, BEING A PART OF THE NW 1/4 OF THE NE 1/4 OF SECTION 1, TOW NORTHEAST CORNER OF TRACT #3 AS SHOWN ON SURVEY PLAT 6-271; THENC THENCE SOUTH 02'31'14" WEST A DISTANCE OF 291.68 FEET; THENCE NORTH 8

THENCE SOUTH 02"11'10" WEST A DISTANCE OF 43.81 FEET; THENCE NORTH 77 ALONG SAID RIGHT OF WAY AS FOLLOWS; NORTH 32'09'22" EAST A DISTANCE DISTANCE OF 183.60 FEET; THENCE NORTH 86°25'31" EAST A DISTANCE OF 117 CONTAINING 3.25 ACRES MORE OR LESS AND SUBJECT TO ANY EASEMENTS OF

<u>TRACT 3B1</u>

PART OF LOT 3 OF THE BOUNDARY SURVEY - LINDSEY, SEXTON, BALL AND MC 271, BEING A PART OF THE NW 1/4 OF THE NE 1/4 OF SECTION 1, TOWNSHIP CORNER OF TRACT #3 AS SHOWN ON SURVEY PLAT 6-271; THENCE NORTH 87' THE POINT OF BEGINNING; THENCE SOUTH 02°31'14" WEST A DISTANCE OF 308.0 DISTANCE OF 521.93 FEET; THENCE NORTH 02"11'10" EAST A DISTANCE OF 40.0 DISTANCE OF 340.58 FEET; THENCE SOUTH 79°20'08" EAST A DISTANCE OF 374 CONTAINING 4.34 ACRES MORE OR LESS AND SUBJECT TO ANY EASEMENTS OF

	901 N. 47th St., Suite 400 Rogers, Arkansas 72756 Crafton Tull architecture engineering surveying 479.636.4838 t www.craftontull.com
	CERTIFICATE OF AUTHORIZATION:
SHEET	
SHEETSHEET TITLENUMBERC-001COVER SHEETC-002SURVEY SHEETC-003DEMO PLANC-101OVERALL SITE PLAN (C)C-102PRELIMINARY PLATC-103PRELIMINARY PLAT TABLESC-104UTILITY PLANC-105OVERALL GRADING PLANC-106GRADING PLAN AC-107EROSION CONTROL PH.1C-108EROSION CONTROL PH.2C-201POND P&P (C)	
C-202 SANITARY SEWER P&P SHT.1 (C) C-203 SANITARY SEWER P&P SHT.2 (C) C-501 EROSION CONTROL DETAILS SHT.1 C-502 EROSION CONTROL DETAILS SHT.2 SD-1 RWU SD-1	
	GATEWAY PLAZA W. HUDSON RD, ROGERS, AR
	Key Plan
FLOOD - IES NO PENDENT EN DONE BY VEY. THE APS CITED	No. Description Date
ICE OF	This document, and the ideas and designs incorporated
ONTGOMERY TRACTS HIGHWAY 62 AND 102, ROGERS , ARKANSAS, AS RECORDED IN PLAT RECORD 6 AT INSHIP 19 NORTH, RANGE 30 WEST, BENTON COUNTY ARKANSAS, DESCRIBED AS COMMENCING AT THE DE NORTH 87°28'51" WEST A DISTANCE OF 271.85 FEET TO THE <u>POINT OF BEGINNING;</u> 30°14'42" WEST A DISTANCE OF 149.59 FEET; THENCE NORTH 79°20'08" WEST A DISTANCE OF 374.42	herein, as an instrument of professional service, is the property of Crafton, Tull & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the written authorization of Crafton, Tull & Associates, Inc.
"13'43" WEST A DISTANCE OF 163.82 FEET TO THE EAST RIGHT OF WAY OF N 8TH STREET; THENCE OF 12.15; NORTH 48'29'28" EAST A DISTANCE OF 180.80 FEET; THENCE NORTH 64'30'23" EAST A 7.60 FEET; THENCE SOUTH 87'22'43" EAST A DISTANCE OF 264.85 FEET TO THE POINT OF BEGINNING. RECORD OR FACT.	Issue DATE: 04/05/2024 S* THIS CONTACT: A.KARR DOCUMENT IS QC by: PRELIMINARY IN PRELIMINARY IN QC Date: NATURE AND IS NOT PRELIMINARY A FINAL, SIGNED PLANS AND SEALED
ONTGOMERY TRACTS HIGHWAY 62 AND 102, ROGERS, ARKANSAS, AS RECORDED IN PLAT RECORD 6 AT PAGE 19 NORTH, RANGE 30 WEST, BENTON COUNTY ARKANSAS, DESCRIBED AS COMMENCING AT THE NORTHEAST "28'51" WEST A DISTANCE OF 271.85 FEET; THENCE SOUTH 02"31'14" WEST A DISTANCE OF 291.68 FEET TO 62 FEET; THENCE NORTH 87"32'36" WEST A DISTANCE OF 265.08 FEET; THENCE NORTH 87"27'43" WEST A 00 FEET; THENCE SOUTH 87"27'18" EAST A DISTANCE OF 270.30 FEET; THENCE NORTH 02"11'10" EAST A 4.42 FEET; THENCE SOUTH 80"14'42" EAST A DISTANCE OF 149.59 FEET TO THE POINT OF BEGINNING. RECORD OR FACT.	COVER SHEET
	C-001





LINEWORK

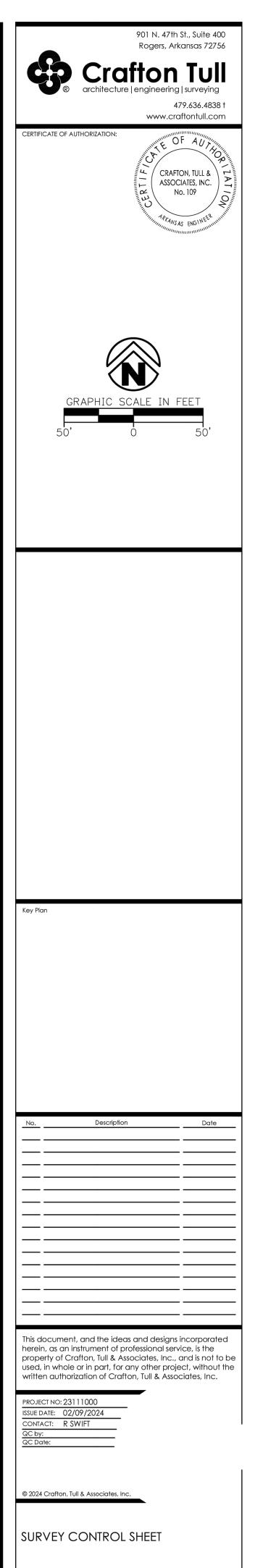
<u>SYMBOLS</u>

BENCHMARK FOUND IRON PIPE/REBAR PRIMARY CONTROL/SET MONUMEN FOUND MONUMENT/ROW AIR CONDITIONER AIR RELEASE VALVE ELECTRIC BOX/PEDESTAL ELECTRIC JUNCTION BOX ELECTRIC METER ELECTRIC TRANSFORMER FIRE DEPARTMENT CONNECTION FIRE HYDRANT GAS METER GAS VALVE GUARD POST LIGHT POLE POWER POLE SANITARY MANHOLE SANITARY SEWER CLEANOUT SIGN SPRINKLER HEAD TELEPHONE PEDESTAL TV PEDESTAL WATER FAUCET WATER METER WATER VALVE DOWN GUY STORM SEWER MANHOLE STORM SEWER PIPE

GRATED INLET

TREE TO BE REMOVED

= = = = = = = = = = = = = = = = = = =	
INTERMEDIATE CONTOUR	—1206—— — -
INDEX CONTOUR	—1205—— — -
SANITARY SEWER LINE (SPECIFY SIZE &	TYPF)
GAS LINE G	(;
WATER LINE (SPECIFY SIZE & TYPE)	
UNDERGROUND TELEPHONE	UGT
UDERGROUND ELECTRIC	
OHE	
UDERGROUND TELEVISION	
OHTV OVERHEAD TELEVISION	OHTV
CHAIN LINK FENCE	O
WOOD FENCE	////
BARBED WIRE FENCE	X
FIBER OPTIC	FO
RIGHT OF WAY	



C-002

(PROJECT CONTROL NOTE

ONLY THE CONTROL POINTS, COORDINATE VALUES, AND ELEVATIONS SHOWN ON THIS SHEET SHALL BE USED ON THIS PROJECT. CRAFTON, TULL & ASSOCIATES SHALL NOT BE RESPONSIBLE FOR ANY CONSEQUENCES OF USING CONTROL POINTS, COORDINATE VALUES OR ELEVATIONS ESTABLISHED OR DERIVED FROM OTHER SOURCES. CRAFTON, TULL & ASSOCIATES, AT ITS SOLE DISCRETION, MAY ALLOW THE USE OF ALTERNATIVE OR ADDITIONAL CONTROL, BY A WRITTEN AMENDMENT TO THIS SHEET SEALED BY THE RESPONSIBLE SURVEYOR. USERS OF THESE CONTROL POINTS SHALL EXERCISE DUE CARE AND GOOD SURVEYING PRACTICE AND IMMEDIATELY NOTIFY THE CRAFTON, TULL & ASSOCIATES PROJECT ENGINEER OF ANY INCONSISTENCIES IN THE OBSERVED COORDINATE VALUES, ELEVATIONS AND DESCRIPTIONS FOR CONTROL POINTS SHOWN ON THIS SHEET.

CONTROL POINT TABLE				
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
127	742580.80	686878.95	1334.45	5/8"RBR
129	742105.20	686546.79	1336.24	5/8"RBR

THE ABOVE CONTROL POINT TABLE ITEM VALUES DO NOT CONSTITUTE A "SURVEYING MEASUREMENT CERTIFICATION" AS DEFINED BY ARKANSAS CODE SECTION 17-48-101 (6) (A) (B) AS OF JULY 2014. THERE IS NO WARRANTY TO THIRD PARTIES AS TO ACCURACY OR CORRECTNESS OF THE NUMERICAL VALUES SHOWN HEREON.

BASIS OF BEARING: ARKANSAS STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NAD 83 HORIZONTAL DATUM AND NAVD 88 VERTICAL DATUM DETERMINED BY GPS OBSERVATION ON THE CITY OF ROGERS CONTROL MONUMENTS USING THEIR PUBLISHED VALUES. BEARING: GRID

DISTANCE: GRID

CONVERGENCE ANGLE: -01°14'09.50" COMBINED SCALE FACTOR: 0.99996246

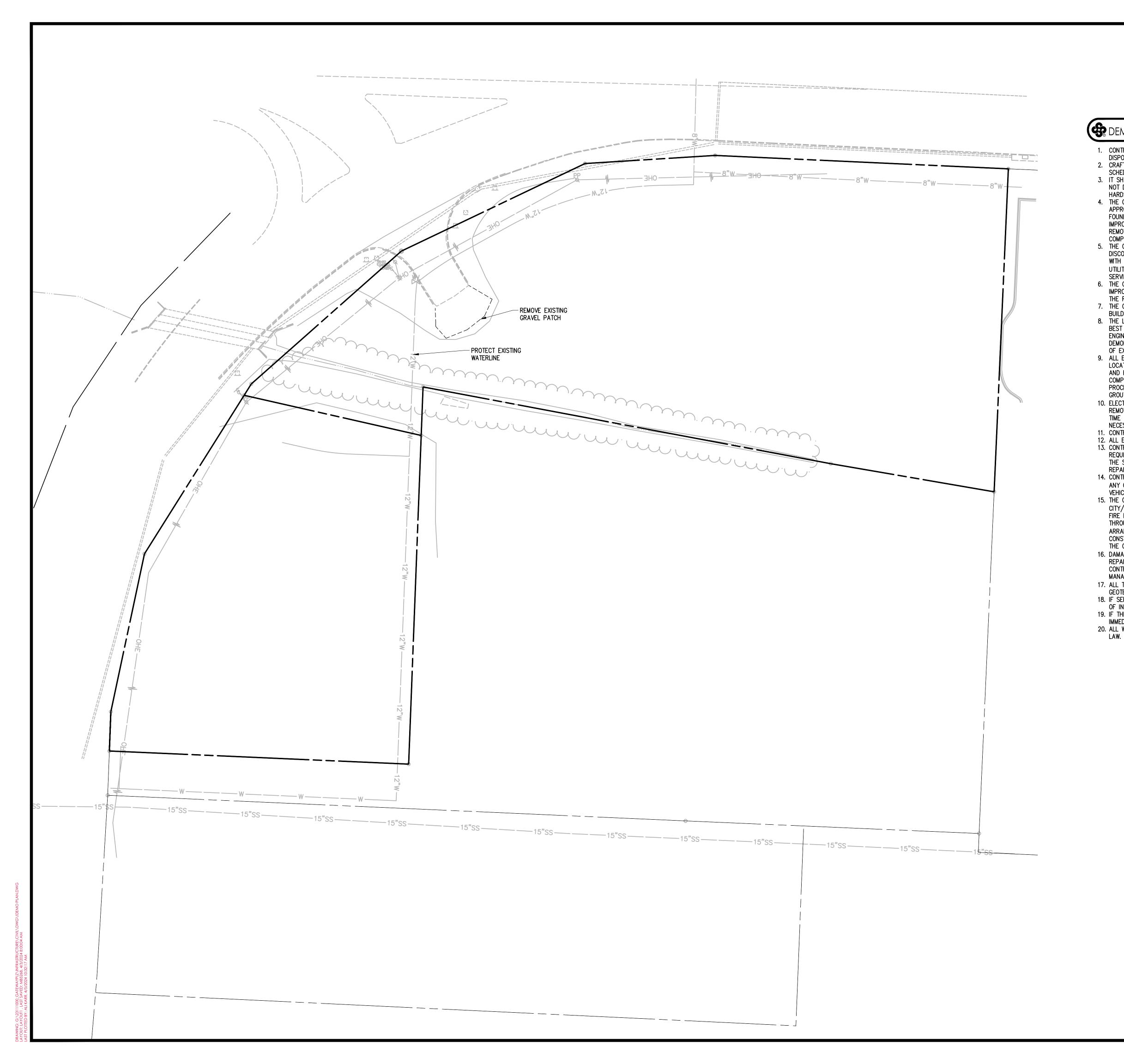
VALUE CALCULATED AT THE FOLLOWING POINT: N 742580.80 E 686878.95 Z 1334.45

PROJECT CONTROL CERTIFICATE:

I HEREBY DECLARE THAT ON THE 9TH DAY OF FEBRUARY, 2024 THE ABOVE CONTROL POINT TABLE ITEM VALUES WERE CHECKED TO FOR CONSISTENCY WITH THE FIELD DATA FOR EACH ITEM PROVIDED BY THE FIELD CREW UNDER MY SUPERVISION. THE CONTROL HORIZONTAL AND VERTICAL DATUM WAS CONFIRMED IN RELATION TO THE RELEVANT LOCAL MUNICIPAL CONTROL SYSTEM. THIS CERTIFICATION IS APPLICABLE ONLY TO THE CONTROL POINT VALUES AS THEY EXISTED AT THE TIME OF IMPLEMENTATION BY THE FIELD CREW UNDER MY SUPERVISION. THIS CERTIFICATE DOES NOT EXTEND TO ANY ITEM OR DEPICTION SHOWN HEREON, OTHER THAN THE CONTROL POINT VALUES SHOWN IN THE CONTROL POINT TABLE.

CRAFTON, TULL, & ASSOCIATES, INC. EGISTERA BY KEVIN J. MONTGOMERY (AGENT) STATE OF ARKANSAS NO. 1460 KEVIN J. MONTGOMERY PS 1460 SIGNATU

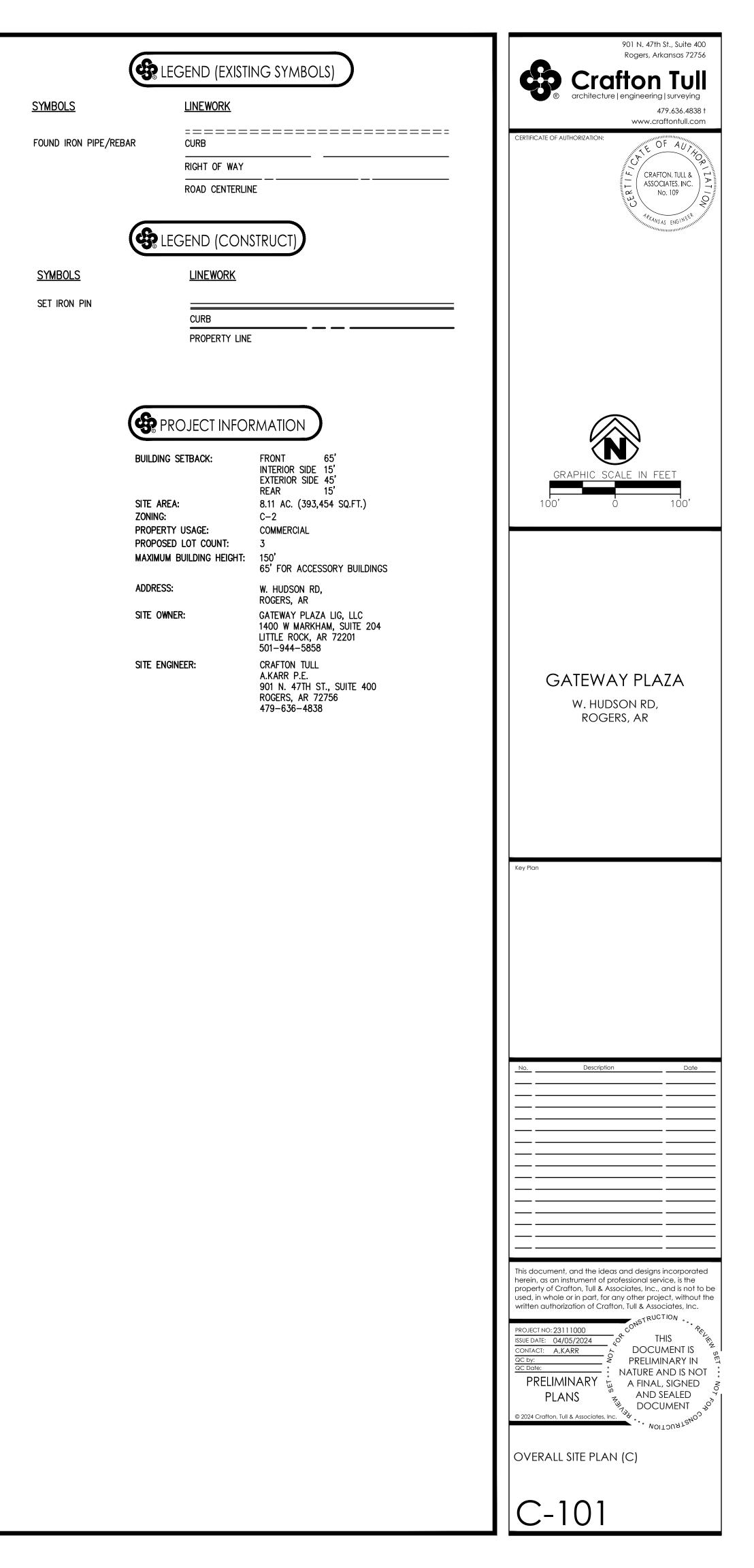
THIS PLAN SHEET DOES NOT CONSTITUTE A BOUNDARY SURVEY OR ALTA/ASPS LAND TITLE SURVEY. EXCEPT FOR THE CONTROL POINT TABLE VALUES, ALL ELSE IS SHOWN FOR INFORMATION ONLY.



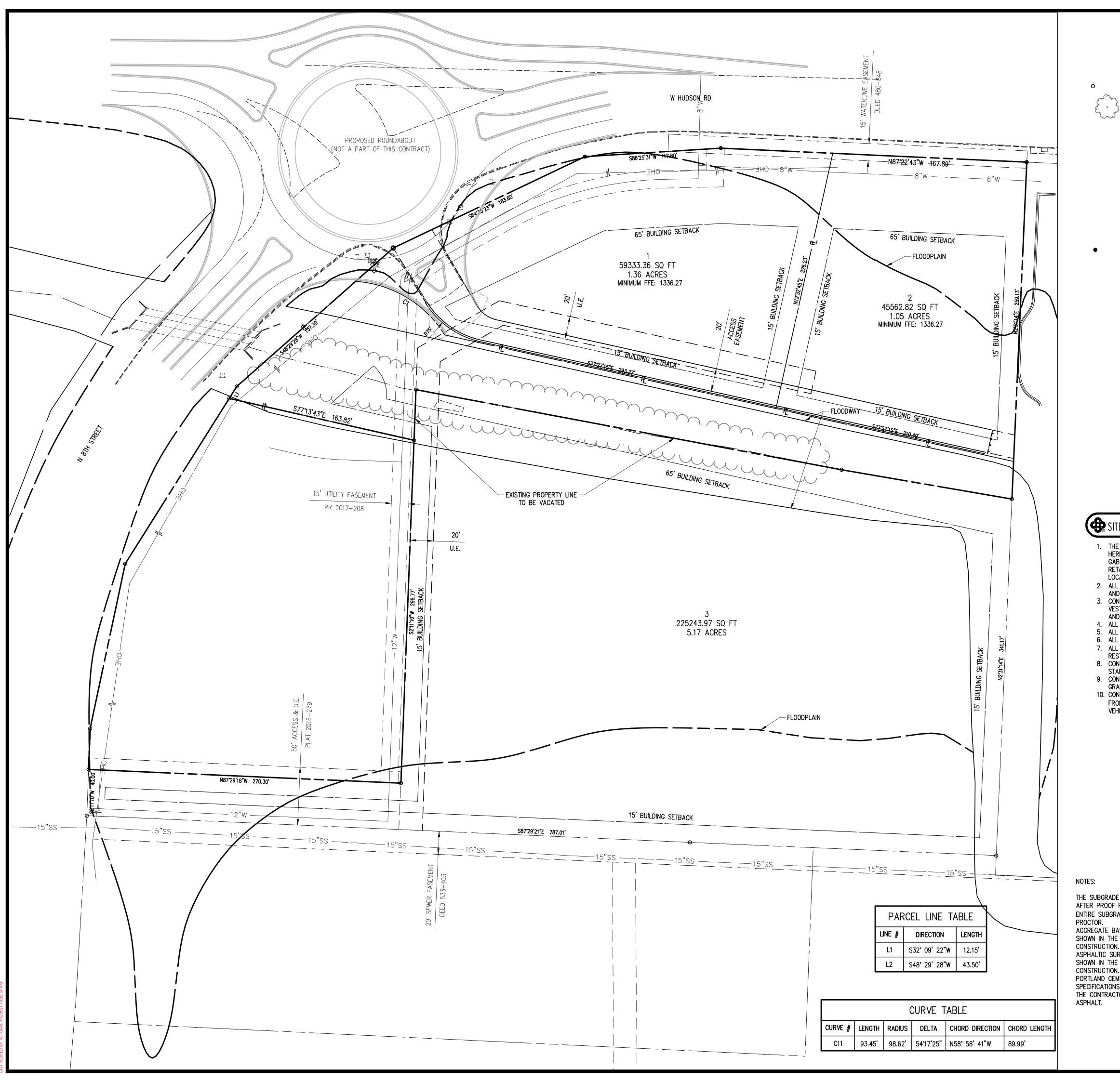
LEGEND (EXISTING SYMBOLS)	901 N. 47th St., Suite 400 Rogers, Arkansas 72756
LINEWORK (TO BE REMOVED)	R architecture engineering surveying 479.636.4838 t
GRAVEL	CERTIFICATE OF AUTHORIZATION:
MOLITION NOTES	ASSOCIATES, INC. No. 109
ATRACTOR SHALL ABIDE BY ALL FEDERAL, STATE, AND LOCAL CODES FOR THE DEMOLITION AND POSAL OF ALL MATERIALS. VFTON, TULL AND ASSOCIATES, INC. SHALL NOT BE LIABLE FOR ANY DEMOLITION PROCEDURES, IEDULING, AND DISPOSAL OF ANY MATERIALS. SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE SURE THAT ADJACENT PROPERTY IS T DAMAGED AND IS ACCESSIBLE AT ALL TIMES, AND THAT CONSTRUCTION DOES NOT CREATE ANY ROMAGED AND DISPOSAL OF ANY MATERIALS. SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE SURE THAT ADJACENT PROPERTY IS T DAMAGED AND IS ACCESSIBLE AT ALL TIMES, AND THAT CONSTRUCTION DOES NOT CREATE ANY ROSVED BY ALL GOVERNING ADJACENT TO THE CONSTRUCTION SITE. C CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSING IN A LOCATION ROVED BY ALL GOVERNING AUTHORITIES, OF ALL STRUCTURES, PADS, WALLS, FLUMES, INDATIONS, PARKING, DRIVES, DRAINAGE, STRUCTURES, UTILITIES, ETC., SUCH THAT THE ROVEMENTS SHOWN ON THE REMAINING PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE IOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE MPACTED FILL MATERIAL PER THE SPECIFICATIONS. CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO THE CONNECTION, REMOVAL AND RELOCATION OF ALL UTILITIES. THE CONTRACTOR SHALL COORDINATE H THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE JITY COMPANY'S FORCES AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR VICES. THE CONTRACTOR SHALL CAP THE EXISTING IRRIGATION SYSTEM IN THE AREAS OF SITE ROVEMENTS. THE CONTRACTOR SHALL CAP THE EXISTING IRRIGATION SYSTEM TO REMAIN SUCH THAT REMAINING SYSTEM SHALL CONTINUE TO FUNCTION PROPERLY. CONTRACTOR IS RESPONSIBLE FOR THE DISCONNECTION OF UTILITY SERVICES TO THE EXISTING LDINGS PRIOR TO DEMOLITION OF THE DISCONNECTION OF UTILITY SERVICES TO THE EXISTING LDINGS PRIOR TO DEMOLITION OF THE DISCONNECTION OF UTILITY SERVICES TO THE EXISTING LDINGS PRIOR TO DEMOLITION OF THE BUILDINGS. LOCATIONS OF ALL EXISTING UTILITIES SHOWN	GRAPHIC SCALE IN FEET
NOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR ONSITE LOCATIONS EXISTING UTILITIES. EXISTING SEWERS, PIPING AND UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT CATION, OR AS THE ONLY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS O PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES. GIVE NOTICE TO ALL UTILITY MPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES AND CAP ALL LINES BEFORE DECEEDING WITH WORK. UTILITIES DETERMINED TO BE ABANDONED AND LEFT IN PLACE SHALL BE OUTED IF UNDER BUILDINGS. CTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC CABLE AND/OR GAS LINES NEEDING TO BE NOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE E SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS DESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE. NTRACTOR MUST PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC. EROSION CONTROL DEVICES ARE TO BE INSTALLED PRIOR TO DEMOLITION. NTRACTOR MAY LIMIT SAW-CUT AND PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS DUIRED AS SHOWN ON THESE CONSTRUCTION PLANS BUT IF ANY DAMAGE IS INCURRED ON ANY OF SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND	GATEWAY PLAZA
AUXINOONDING TAXEMENT, ETC. THE CONTRACTOR STREE BE REDICTIONABLE FOR THIS REMOVAE AND TAR. JITRACTOR SHALL MAINTAIN ALL EXISTING PARKING, SIDEWALKS, DRIVES, ETC. CLEAR AND FREE FROM (CONSTRUCTION ACTIVITY AND/OR MATERIAL TO ENSURE EASY AND SAFE PEDESTRIAN AND ICULAR TRAFFIC TO AND FROM THE SITE. CONTRACTOR SHALL COORDINATE WATERMAIN WORK WITH THE FIRE DEPARTMENT AND THE Y/COUNTY UTILITY DEPARTMENT TO PLAN WATERMAIN IMPROVEMENTS AND TO ENSURE ADEQUATE E PROTECTION IS CONSTANTLY AVAILABLE TO THE SITE THROUGHOUT THIS SPECIFIC WORK AND YOUGH ALL PHASES OF CONSTRUCTION. CONTRACTOR WILL BE RESPONSIBLE FOR VANCING/PROVIDING ANY REQUIRED WATERMAIN SHUT-OFFS WITH THE CITY/COUNTY DURING ISTRUCTION. ANY COSTS ASSOCIATED WITH WATERMAIN SHUT-OFFS WILL BE THE RESPONSIBILITY OF CONTRACTOR AND NO EXTRA COMPENSATION WILL BE PROVIDED. AGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT CONTRACTOR'S EXPENSE. AVAIRS SHALL RESTORE DAMAGED ITEMS TO EQUAL OR BETTER THAN, EXISTING CONDITIONS. ITRACTOR IS RESPONSIBLE FOR DOCUMENTING ALL EXISTING DAMAGE AND NOTIFYING CONSTRUCTION AGE PRIOR TO CONSTRUCTION START. TRENCHES AND/OR EXCAVATED AREAS SHALL BE FILLED/TESTED IN ACCORDANCE WITH THE DIECHNICAL ENGINEERING REPORT. SEPTIC TANKS ARE FOUND PRESENT WITHIN THE LIMITS OF DISTURBANCE THEY SHALL BE DISPOSED IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL LAWS. THE CONTRACTOR FINDS ANY UNDERGROUND TANKS ON SITE THEY SHALL CONTACT THE ENGINEER EDIATELY. WELLS SHALL BE CAPPED AND CLOSED IN ACCORDANCE WITH APPLICABLE STATE AND FEDERAL A. ()	Key Plan
	No. Description Date
	This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Crafton, Tull & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the written authorization of Crafton, Tull & Associates, Inc. PROJECT NO: 23111000 ISSUE DATE: 04/05/2024 CONTACT: A.KARR GC by: GC Date: PRELIMINARY PLANS © 2024 Crafton, Tull & Associates, Inc. WOILDNULSNO DEMO PLAN
	C-003



AWING: G:\23111000_GATEWAYPIZ\INFRASTRUCTURE\CIVIL\DWG\!OVERALI SITE PLAN.E YOUT: LAYOUT1 , LAST SAVED: AE5162, 4/5/2024 10:28:08 AM

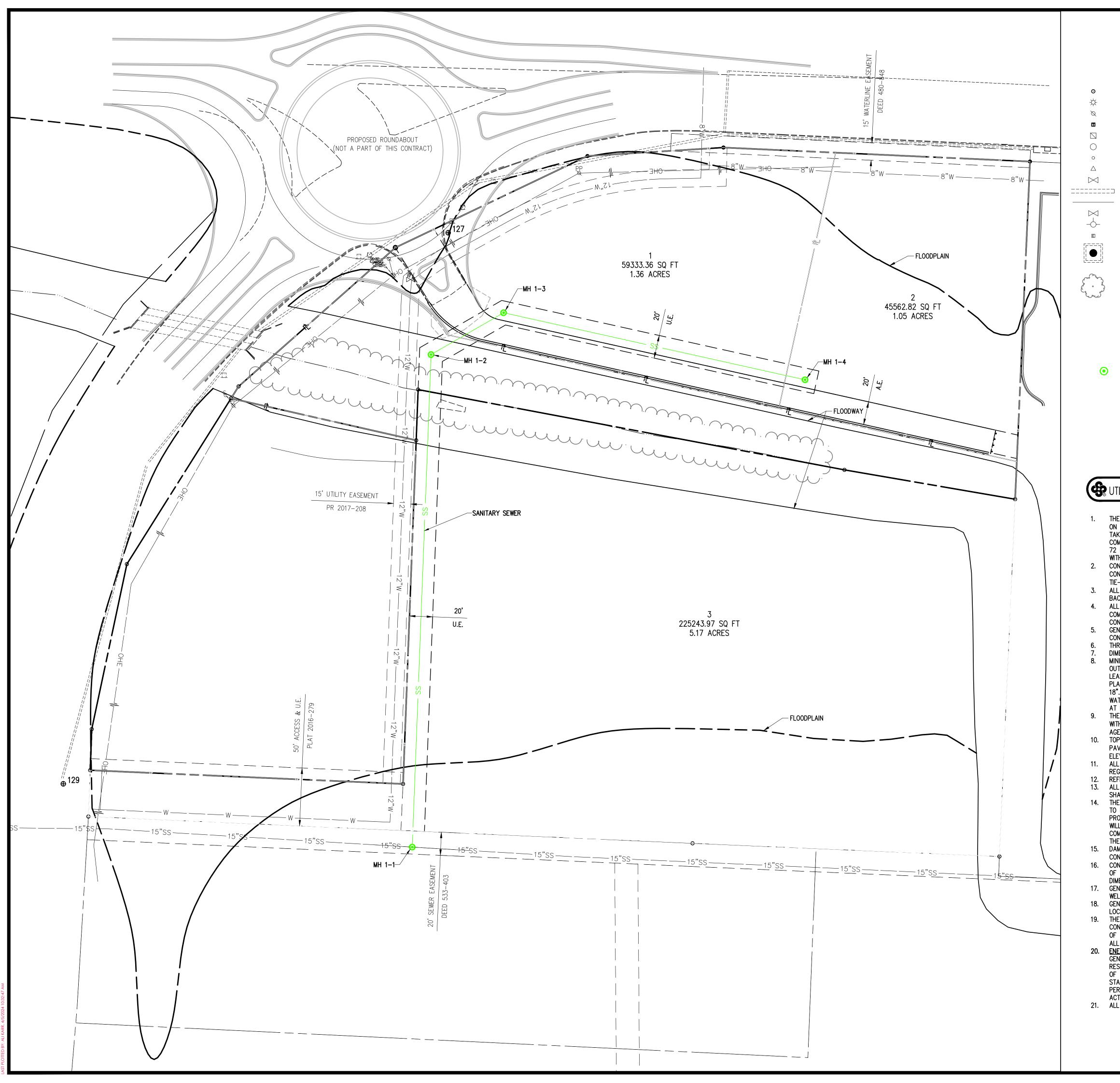


O



G: G: \23111000_GATEWAYPLZ\INFRASTRUCTURE\CIVIL\DWG\!SITE PLAT SITE PLAN EXHBIT A , LAST SAVED: AE51 62, 4/5/2024 10:28:33 AM

	EGEND (EXIST		901 N. 47th St., Suite 400 Rogers, Arkansas 72756
SYMBOLS	LINEWORK		© Crafton Tull architecture engineering surveying
STINDOLS			479.636.4838 t www.craftontull.com
FOUND IRON PIPE/R	=====	=======================================	CERTIFICATE OF AUTHORIZATION:
TREE	CURB 		CRAFTON, TULL & ASSOCIATES, INC. No. 109
	ROAD CENTERLI	NE	No. 109
			and SAS ENGLIST
	EGEND (CON	ISTRUCT	
<u>SYMBOLS</u>	LINEWORK		
SET IRON PIN	CURB		
	BUILDING SET	BACK	
	PROPERTY LIN	E	
	PROJECT INFO	RMATION	GRAPHIC SCALE IN FEET
	BUILDING SETBACK:	FRONT 65' INTERIOR SIDE 15' EXTERIOR SIDE 45' REAR 15'	40' 0 40'
	SITE AREA: ZONING:	8.11 AC. (393,454 SQ.FT.) C–2	
	PROPERTY USAGE: PROPOSED LOT COUNT:	COMMERCIAL 3	
	MAXIMUM BUILDING HEIGHT:	150' 65' FOR ACCESSORY BUILDINGS	
	ADDRESS:	W. HUDSON RD, ROGERS, AR	
	SITE OWNER:	GATEWAY PLAZA LIG, LLC 1400 W MARKHAM, SUITE 204 LITTLE ROCK, AR 72201 501–944–5858	GATEWAY PLAZA
	SITE ENGINEER:	CRAFTON TULL A.KARR P.E.	W. HUDSON RD, ROGERS, AR
		901 N. 47TH ST., SUITE 400 ROGERS, AR 72756 479–636–4838	ROGERS, AR
ITE NOTES			
EREIN, INCLUDING BUT N ABION WALLS, ETC., GRE ETAINING WALL DATA SH DCATION AND ELEVATION LL WORK AND MATERIALS ND CODES AND OSHA ST ONTRACTOR SHALL REFE	NOT LIMITED TO, SEGMENTAL RE ATER THAN FORTY-EIGHT INCHE HOWN OR REFERENCED HEREIN S IS ONLY. S SHALL COMPLY WITH ALL CITY, FANDARDS. R TO ARCHITECTURAL PLANS FOL	TAINING WALL SHOWN OR REFERENCED TAINING WALLS, MASS GRAVITY WALLS, IS IN HEIGHT, SHALL BE BY OTHERS. ANY HALL BE FOR COORDINATION OF THE WALL /COUNTY/STATE/FEDERAL REGULATIONS R EXACT LOCATIONS AND DIMENSIONS OF	Key Plan
ND EXACT UTILITY ENTRA LL CURB DIMENSIONS AN LL PAVEMENT MARKINGS LL WORK SHALL BE DONI LL CURB/SIDEWALK/HANI ESTRICTIVE CODES, WHICH ONTRACTOR SHALL ENSU TART.	ANCE LOCATIONS. D RADII ARE TO BACK OF CURB DIMENSIONS FACE OF CURB UNL E IN STRICT ACCORDANCE WITH DICAP RAMP DESIGNS SHALL COI HEVER IS MORE RESTRICTIVE. RE ALL NECESSARY PERMITS AR	ESS OTHERWISE NOTED. THE PROJECT SPECIFICATIONS. NFORM TO ADA STANDARDS OR LOCAL E OBTAINED PRIOR TO CONSTRUCTION	
RADE AND ALIGNMENT.		CRETE, AND PAVEMENT TO EXISTING IN EWALKS, DRIVES, ETC. CLEAR AND FREE	
	ACTIVITY AND/OR MATERIAL TO	ENSURE EASY AND SAFE PEDESTRIAN AND	
	XX" ACHM SURFACE COU	RSE —	No. Description Date
KK	XX" BINDER XX" AGGREGATE		
12	BASE COURSE		
	<u> 2000 00000000000000000000000000000000</u>	<u> </u>	
	XX" SELECT MATERIAL AS DIREC SINEER OR GEO-TECHNICAL REPO		
DI INE ENG	HEAVY DUTY ASPHAL		
			··
F ROLLING WITH A LOADE	ED TANDEM AXLE DUMP TRUCK	ANCE WITH THE CONTRACT SPECIFICATIONS. AND PREPARING DEEP SUBGRADE DEFICIENCIES, THE NIFORMLY COMPACTED TO AT LEAST 95% OF MODIFIED	This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Crafton, Tull & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the written authorization of Crafton, Tull & Associates, Inc.
BASE COURSE MIXTURES	SHALL CONFORM WITH THE GRA	DATION, COMPACTION AND OTHER REQUIREMENTS	PROJECT NO: 23111000 ISSUE DATE: 04/05/2024 CONSTRUCTION ISSUE DATE: 04/05/2024 CONSTRUCTION ISSUE DATE: 04/05/2024 CONSTRUCTION ISSUE DATE: 04/05/2024
N. URFACE AND BINDER CO	URSES SHALL CONFORM WITH TH	DARD SPECIFICATIONS FOR ROAD AND BRIDGE IE GRADATION, COMPACTION AND OTHER REQUIREMENTS	QC by: PRELIMINARY IN QC Date: NATURE AND IS NOT
IE CONTRACT SPECIFICAT	TIONS OR WITH THE LOCAL STAN	DARD SPECIFICATIONS FOR ROAD AND BRIDGE REQUIREMENTS SHOWN IN THE CONTRACT	PRELIMINARY A FINAL, SIGNED
NS OR WITH THE LOCAL	STANDARD SPECIFICATIONS FOR	ROAD AND BRIDGE CONSTRUCTION. CONCRETE GUTTER WHERE THE GUTTER CONTACTS	PLANS AND SEALED DOCUMENT ^(a) 2024 Crafton, Tull & Associates, Inc.
			PRELIMINARY PLAT
			C-102

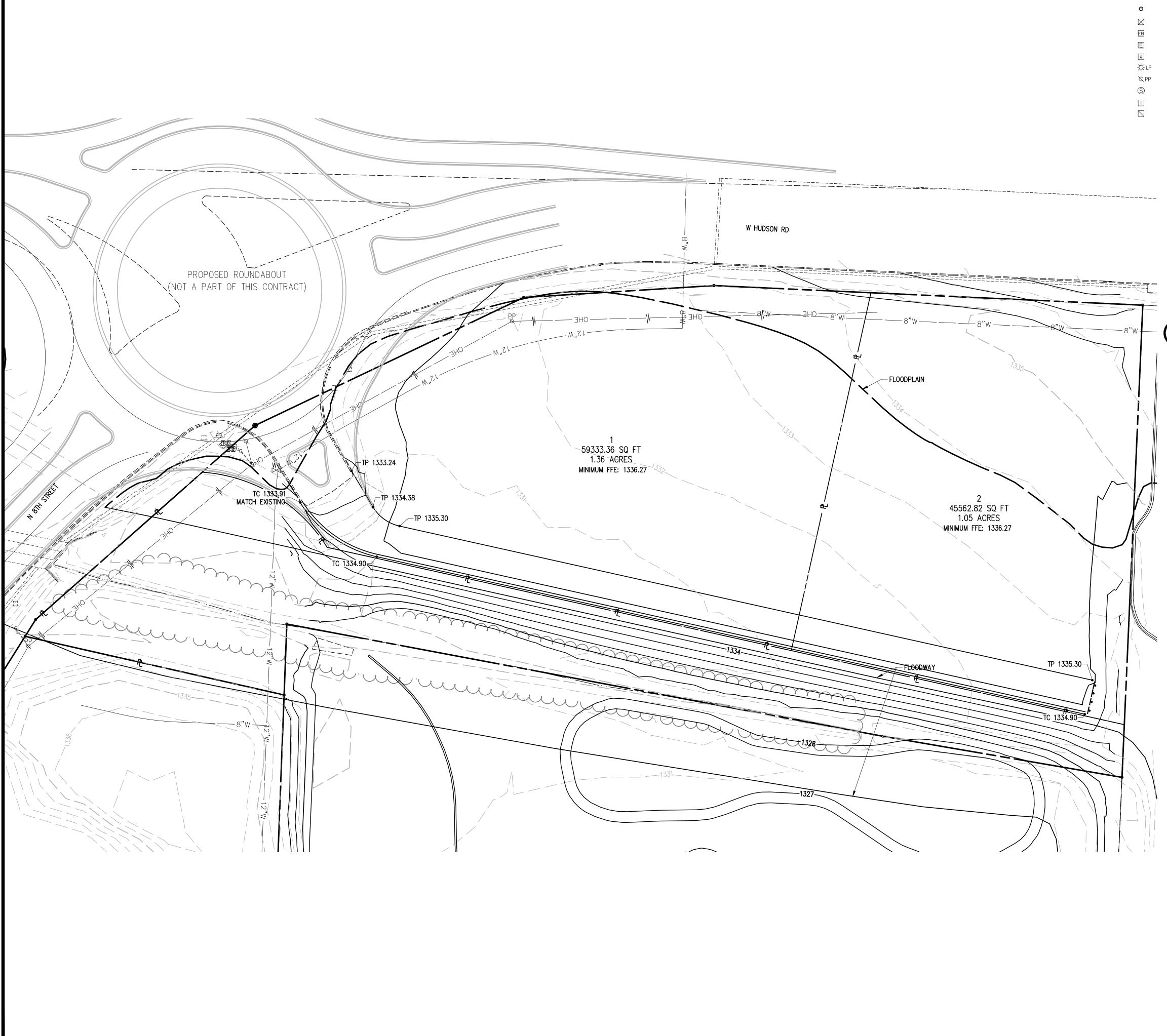


(Gr	EGEND (EXISTING SYMBOLS)	901 N. 47th St., Suite 400 Rogers, Arkansas 72756 Crafton Tull
SYMBOLS	LINEWORK	
FOUND IRON PIN LIGHT POLE POWER POLE TELEPHONE PEDESTAL TV PEDESTAL MANHOLE SANITARY SEWER CLEANOUT GAS METER GAS VALVE STORM SEWER PIPE DOWN GUY WATER VALVE FIRE HYDRANT ASSEMBLY ELECTRIC PEDESTAL	EINENT	CERTIFICATE OF AUTHORIZATION:
DROP INLET		
TREE		
SYMBOLS	LEGEND (CONSTRUCT)	
SYMBOLS MANHOLE	LINE WORK EASEMENT ASPHALT SANITARY SEWER LINE RIGHT OF WAY PROPERTY LINE	
UTILITY NOTES		GATEWAY PLAZA W. HUDSON RD, ROGERS, AR
ON RECORDS OF THE VARIOUS UTILITY TAKEN IN THE FIELD. THE INFORMATIC COMPLETE. THE CONTRACTOR MUST OF 72 HOURS BEFORE ANY EXCAVATION T WITHIN THE WORK ZONE. CONTRACTOR SHALL NOTIFY THE UTILIT CONNECTING TO ANY EXISTING FACILITI TIE-INS/CONNECTIONS WITH ALL UTILIT ALL UNDERGROUND LINES SHALL BE IN BACKFILLING. ALL NECESSARY INSPECTIONS AND/OR COMPANIES SHALL BE PERFORMED PRI- CONNECTION OF SERVICE. GENERAL CONTRACTOR IS TO COORDIN CONSECTION OF SERVICE. GENERAL CONTRACTOR IS TO COORDIN CONSTRUCTION, ADJUSTMENT, OR RELC THRUST BLOCKING SHALL BE PROVIDED DIMENSIONS SHOWN ARE TO CENTERLIN MINIMUM HORIZONTAL SEPARATION BET OUTSIDE WALL OF THE SANITARY SEWE LEAST TEN FEET. WHERE WATERLINES OF PLACED ABOVE THE SEWER WITH A MII 18". IF IT IS NOT POSSIBLE TO CONFO WATERLINE SHALL BE ENCASED IN WAT AT LEAST TEN FEET EITHER SIDE OF T THE CONTRACTOR SHALL INCLUDE IN T WITH THE TESTING OF THE WATER AND AGENCIES. TOPS OF EXISTING MANHOLES SHALL BE PAVEMENT ELEVATIONS, AND MANHOLE ELEVATIONS WITH WATER TIGHT LIDS. ALL TRENCHING, PIPE LAYING, AND BA REGULATIONS. REFER TO BUILDING PLANS FOR SITE L ALL MATERIALS, CONSTRUCTION, AND I SHALL BE PER THE SPECIFICATIONS OF THE CONTRACTOR SHALL COORDINATE TO PLAN THE IMPROVEMENTS TO THE PROTECTION IS CONSTRUCTION, AND I SHALL BE PER THE SPECIFICATIONS OF THE CONTRACTOR SHALL COORDINATE TO PLAN THE IMPROVEMENTS TO THE PROTECTION IS CONSTRUCTION. ALL THE RESPONSIBLE FOR ARRANGING COMPANY DURING CONSTRUCTION. ALL THE RESPONSIBLE FOR ARRANGING COMPANY DURING CONSTRUCTION. ALL THE RESPONSIBLE FOR ARRANGING CONTRACTOR SHALL REFER TO ARCHIT OF VESTIBULES, SLOPE PAVING, SIDEW, DIMENSIONS, AND EXACT UTILITY ENTR. GENERAL CONTRACTOR SHALL INCLUDE IN E CONDITION OF ALL OF THE UNDERGROU OF THE NOCESSARY/REQUIRED WATER ALL OTHER INFORMATION REQUIRED FO ENERGIZED ELECTRICAL LINE SAFETY. Y GENERAL CONTRACTORS, AND SUBCOM RESPONSIBLE FOR FAMILIARIZING THEM OF ARKANSAS CODES AR ST § 11–5– STATE CODES PERTAINING TO ADVANCE PERSONNEL, INCLUDING THE GENERAL ACTIVITY IN CLOSE PROXIMITY TO ANY	ISTALLED, INSPECTED, AND APPROVED PRIOR TO CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY OR TO ANNOUNCED BUILDING POSSESSION AND THE FINAL ATE WITH APPROPRIATE UTILITY COMPANIES PRIOR TO CATION OF EXISTING UTILITES.) AT ALL BENDS, TEES, AND FIRE HYDRANTS. WE OF PIPE OR FITTING. WEEN THE OUTSIDE WALL OF THE WATERLINE AND THE TR LINE OR SANITARY SEWER MANHOLE SHALL BE AT CROSS SANITARY SEWERS THE WATERLINE SHALL BE NIMUM VERTICAL SEPARATION, OUTSIDE-TOOUTSIDE, OF RM TO THESE DIMENSIONS OR DEFINED PLACEMENT, THE TERTIGHT PIPE WITH SEALED WATERTIGHT ENDS EXTENDING HE CROSSING. THE BID PRICE ALL MATERIAL AND LABOR ASSOCIATED D SEWER LINES REQUIRED BY THE LOCAL AND/OR STATE SE RAISED AS NECESSARY TO BE FLUSH WITH FINISHED S IN UNPAVED AREAS TO BE 1" ABOVE FINISHED GROUND CKFILLING SHALL BE IN ACCORDANCE WITH FEDERAL JGHTING AND ELECTRICAL PLAN. NSPECTION FOR WATER AND SANITARY SEWER LINES F THE APPROPRIATE AGENCY. WITH THE FIRE DEPARTMENT AND THE WATER COMPANY WATER MAINS AND TO ENSURE ADEQUATE FIRE E TO THE SITE THROUGHOUT THE PROJECT. CONTRACTOR ANY REQUIRED WATER MAIN SHUT-OFFS WITH THE WATER COSTS ASSOCIATED WITH WATERMAIN SHUT-OFFS WILL BE TOR: NO ADDITIONAL COMPENSATION WILL BE PROVIDED. JESIGNATED TO REMAIN WILL BE REPLACED AT ECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS ALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING ANCE LOCATIONS. PONSIBLE FOR ALL TAP AND THE DUALS. VERIFY YOVIDE NYLON PULL CORDS INSIDE THE CONDUIT. JD PRICE THE DALLY RECORD KEEPING OF THE RECORD JND UTILITES, CONSTRUCTION STAKE-OUT, PREPARATION AND SEWER RECORD DRAWING STO BE SUBMITTED, AND R OBTINING PERMITS AND RELEASE OF BONDS. MARNINGS, AND ADVANCED NATHE PLANS, VERIFY YOVIDE NYLON PULL CORDS INSIDE THE CONDUIT. JD DRICE THE DALLY RECORD KEEPING OF THE RECORD JND UTILITIES, CONSTRUCTION STAKE-OUT, PREPARATION AND SEWER RECORD DRAWINGS TO BE SUBMITTED, AND R OBTINING PERMITS AND RELEASE OF BONDS. MARNINGS, AND ADVANCENT NOTICES; ALL OWNERS, TRACTORS ASSOCIATED WITH THIS PROJECT SHALL BE SELVES WITH, COMPLYING WITH, AND THE EN	No. Description Date Image: Constraint of the ideas and designs incorporated therein, as an instrument of professional service, is the property of Crafton, Tul & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the section of Crafton, Tul & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the section of Crafton, Tul & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the section of Crafton, Tul & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the section of the sec

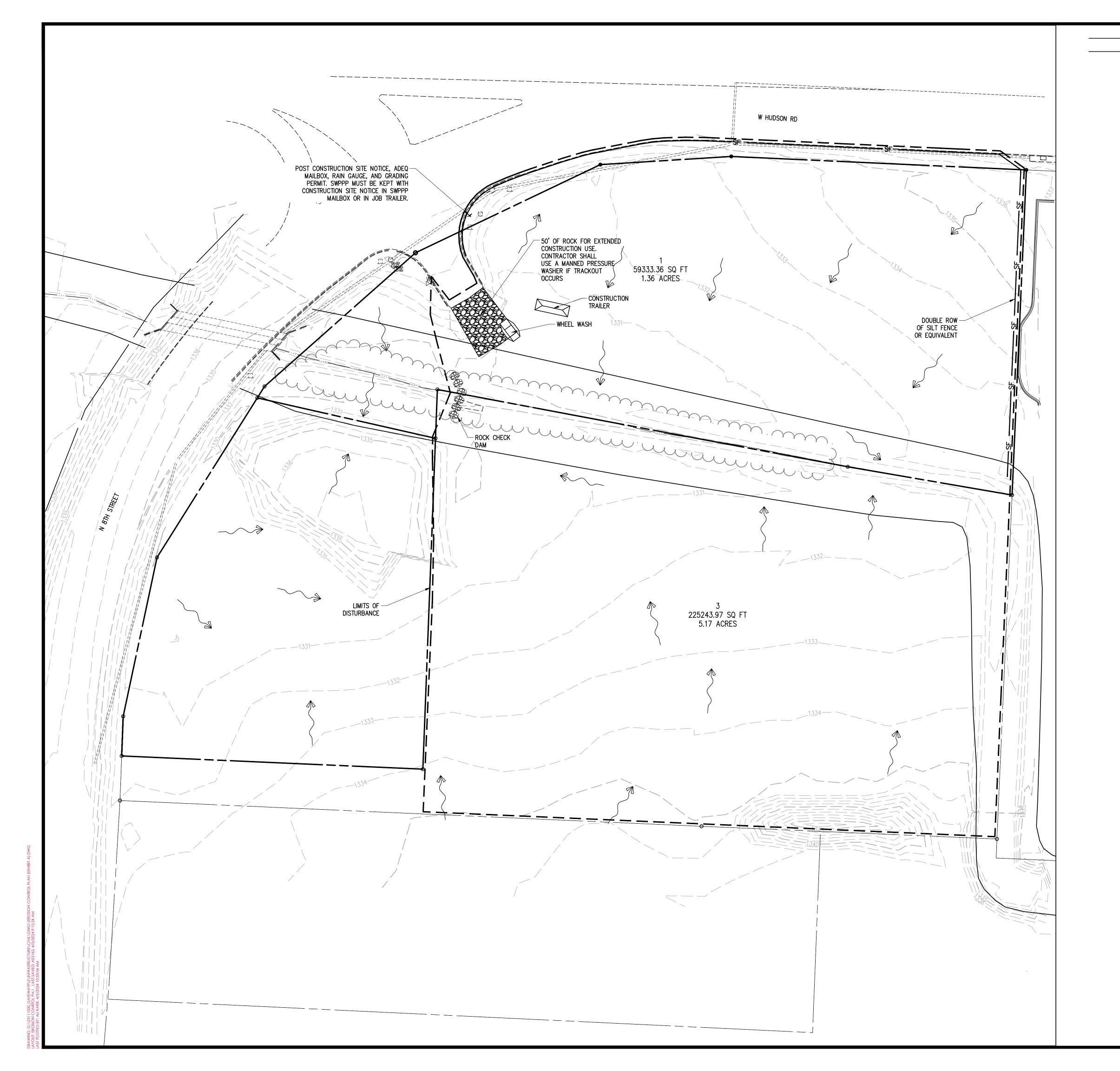


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		901 N. 47th St., Suite 400
	GEND (EXISTING SYMBOLS)	Rogers, Arkansas 72756
SYMBOLS	LINEWORK	
FOUND IRON PIPE/REBAR ELECTRIC BOX/PEDESTAL	EASEMENT	
ELECTRIC JUNCTION BOX ELECTRIC METER	======================================	CRAFTON, TULL &
ELECTRIC TRANSFORMER	INTERMEDIATE CONTOUR	CRAFTON, TULL & ASSOCIATES, INC. No. 109
LIGHT POLE POWER POLE	INDEX CONTOUR	The ANSAS ENGINEER WINNING
SANITARY MANHOLE	RIGHT OF WAY	
TELEPHONE PEDESTAL TV PEDESTAL	ROAD CENTERLINE	
	ND (CONSTRUCT)	
SYMBOLS	LINEWORK	
TC XX.XX TOP OF CURB	CURB	
TP XX.XX TOP OF PAVEME		
	INDEX CONTOUR	
	PROPERTY LINE	
		GRAPHIC SCALE IN FEET
		40' 0 40'
ING AND DRAINAGE NOTE		
	OF ANY RETAINING WALL SHOWN OR REFERENCED HEREIN, INCLUDING	
LIMITED TO, SEGMENTAL RETAINING W IGHT INCHES IN HEIGHT, SHALL BE BY	VALLS, MASS GRAVITY WALLS, GABION WALLS, ETC., GREATER THAN Y OTHERS. ANY RETAINING WALL DATA SHOWN OR REFERENCED	W. HUDSON RD, ROGERS, AR
SHALL BE FOR COORDINATION OF THE ER/CONTRACTOR SHALL BE SOLELY RE	WALL LOCATION AND ELEVATIONS ONLY. SPONSIBLE FOR OBTAINING AND PROVIDING SEPARATE AND	
•	CTIONS, AND CERTIFICATIONS BY A REGISTERED PROFESSIONAL SHALL ALSO APPLY TO ANY ASSOCIATED AND NECESSARY PUBLIC TO PEDESTRIAN SAFETY RAILS	
ATION AND/OR ELEVATION OF EXISTING	UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS	
BE RELIED ON AS BEING EXACT OR COI ES AT LEAST 72 HOURS BEFORE ANY E	MPLETE. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF EXISTING	
K SHALL BE DONE IN STRICT ACCORDA	UTILITIES MUST BE LOCATED PRIOR TO GRADING START. NCE WITH THE PROJECT SPECIFICATIONS. SLOPE OR FLATTER UNLESS OTHERWISE NOTED.	
XISTING STRUCTURES TO REMAIN ARE [DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S HE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING	Key Plan
N OR BETTER. RM SEWER PIPE CONNECTIONS TO STRU	ICTURES SHALL BE GROUTED TO ENSURE CONNECTION AT STRUCTURE	
N TO INVERT OUT.	ES SHALL HAVE A SMOOTH UNIFORM POURED MORTAR INVERT FROM	
D ACCORDINGLY WHEN IN PAVED AND ⁻	PIPES SHALL MEET HEAVY DUTY TRAFFIC (H20) LOADING AND BE TRAFFIC AREAS. S SHALL BE FLUSH WITH THE PAVEMENT AND SHALL HAVE TRAFFIC	
	NPAVED AREAS SHALL BE 1" ABOVE FINISH GRADE. LIDS SHALL BE	
D. THE CONTRACTOR SHALL ADHERE 1	ROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN TO ALL TERMS AND CONDITIONS AS OUTLINED IN THE GENERAL NPDES CHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.	
	NG OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL TO FINAL GRADE.	
EXISTING TOPOGRAPHY AS SHOWN ON F	POGRAPHIC SURVEY BY LAND SURVEYORS. IF CONTRACTOR DOES NOT PLANS, CONTACT ENGINEER IMMEDIATELY.	
OUT ALL PHASES OF CONSTRUCTION.	INAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES,	No. Description Date
AVING, SIDEWALKS, EXIT PORCHES, TRUE E LOCATIONS.	CK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT UTILITY	
PLANS AND SPECIFICATIONS.	NS AND SLABS SHALL BE IN ACCORDANCE WITH ARCHITECTURAL TED AND REPAIRED AS NEEDED, AND EXISTING PIPES TO BE CLEANED	
REMOVE ALL SILT AND DEBRIS.	TING PAVEMENT AS NECESSARY TO ENSURE A SMOOTH FIT AND	
DUS GRADE. TOR SHALL MAINTAIN ALL EXISTING PAI	RKING, SIDEWALKS, DRIVES, ETC. CLEAR AND FREE FROM ANY	
M THE SITE.	ENSURE EASY AND SAFE PEDESTRIAN AND VEHICULAR TRAFFIC TO	
DESIGN AND PLACEMENT OF A FRENCH		
H INDIVIDUAL BUILDING OR LÒT IŃ THIS HED AND NOTED, THE ULTIMATE RESPO	S SUBDIVISION. WHEN A MINIMUM BUILDING FLOOR ELEVATION IS NOT DNSIBILITY FOR THE PROPER GRADING OF EACH INDIVIDUAL LOT OR	This document, and the ideas and designs incorporated
「TWELVE INCHES (12") ABOVE THE FIN	TYPICALLY, THE MINIMUM FINISH FIRST FLOOR ELEVATIONS SHOULD BE IISHED TOP OF STREET CURB ELEVATION ASSOCIATED WITH EACH	herein, as an instrument of professional service, is the property of Crafton, Tull & Associates, Inc., and is not to be
L LOT OR PARCEL OR AS REQUIRED B	Y LOCAL AND STATE CODES.	used, in whole or in part, for any other project, without the written authorization of Crafton, Tull & Associates, Inc.
		ISSUE DATE: 04/05/2024 5 THIS
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4	SITE STABILIZATION AND CERTIFICATION	PRELIMINARY A FINAL, SIGNED
CRAF	TON TULL CANNOT CERTIFY THE SITE AS COMPLETE IN ORDER	PLANS AND SEALED DOCUMENT S (© 2024 Crafton, Tull & Associates, Inc.
το ο	OBTAIN THE CERTIFICATE OF OCCUPANCY UNTIL ALL URBED AREAS RELATED TO THE CONSTRUCTION OF THE	
PROJ	JECT, BOTH ONSITE AND OFFSITE, HAVE BEEN STABILIZED PER PLANS AND SPECIFICATIONS AND ALL REQUIREMENTS SPELLED	OVERALL GRADING PLAN
OUT	IN PERMITS ISSUED BY THE STATE AND LOCAL AUTHORITIES E BEEN MET.	
	. DELIY WE	C-104



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	® architecture engineering surveying 479.636.4838 t	SYMBOLS LINEWORK	<u>SY</u>
<form></form>	CERTIFICATE OF AUTHORIZATION:	ELECTRIC BOX/PEDESTAL ====================================	ELI ELI ELI ELI PO SA TE
HEERIN, INCLUDING BUT NOT LIMITED TO, SECMENTAL RETAINING WALLS, MASS GRAVITY WALLS, MAY BERNING WALL DRAWN WALL DATA STORMART INVESTIGATION SHALL BE FOR CORPORATION OF THE WALL AREA MALLS, DEC., GRAVET HAN FORT-ZENERADE HEERIN SHALL BE FOR CORPORATION OF THE WALL AND INDEPENDENT RETAINING WALL DESIGNS, INSPECTIONS, AND CERTIFICATIONS BY A REGISTREED PROFESSIONAL BUCKIERE OF THE HAND CAAT'ON UTUL. HISS SHALL ALSO APPLY TO ANY ASSOCIATED WALLSO APPLY TO AND CONTACT THE SALE OF ALSY REPORTS. AND CERTIFICATIONS BY A REGISTREED PROFESSIONAL BUCKIERE OF THE MAIL CAAT'ON UTUL HISS SHALL ALSO APPLY TO ANY ASSOCIATED WALLSO APPLY TO AND/OR ELEVATION OF EXERTING WITHER TO, PEEST RIAN SAFETY PROFESSIONAL BUCKIES AND OF EXERSING WITHER CASSORDEL, MEASTREMANTS TAKCHIN THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTULITY COMPANIES AT LEAST 72 HOURS EFFORE ANY ALL LUTLIES MUST BE LOCATED PROR TO CRUMMES THAT TO THE ASSOCIATED ON THE CONTRACTOR SERVISION THIS OF BERSING AND CEXTRING WITHER TO RESET OF TOTATIONS. 4. ALL WORK STRUCTURES TO REMAR AND CASSORDER ON THE CASSORDER ON THE CONTRACTOR SERVISION TO BE REALE ON THE REMARKS ON STRUCTORS. 4. ALL WORK SERVISION TO REPARA ADVO REMARKS AND CEXTREM STRUCTURE AS SUCCESS OF TOTATIONS. 4. ALL WORK SERVISION TO REPARA ADVO REPLACE THE EXSTINCE STRUCTURE AS SUCCESS OF TOTATIONS. 4. ALL WORK REVER MEMORY TO REMARK ADVORMENT CONTROL CASSON TO THE CASSON TO THE REALESS. 4. ALL STORM STRUCTURES TO REMARK ADVORMENT AND THAT PLACESS. 5. TRUCTURE IS WATERINGT IN ALL PROFENATION ADVORMENT CONTROL MEASURES THAT AND THE CARESON AND SERVISES OF THE ADVORMENT AND THAT AND THE CASSON AND SERVISES OF TOTAL TO THE MEASURE ADVORMENT AND THAT AND THE CARESON AND SERVISES OF TOTAL TO THE MEASURE ADVORMENT AND THAT AND THE CARESON AND SERVISES OF TOTAL TO THE MEASURE ADVORMENT AND THAT ADVORMENT AND THAT ADVORMENT AND THAT AND THE CARESON AND SERVISES OF TOTAL THAT ADVORMENT AND THAT ADVORMENT AND THAT AND THE CARE ADVORMENT AND THAT AND THAT AND THAT AND THE CARE ADV	GRAPHIC SCALE IN FEET	SYMBOLS LINEWORK TC XX.XX TOP OF CURB CURB TP XX.XX TOP OF PAVEMENT 1206 INTERMEDIATE CONTOUR 1205	
LOADNG AND BE INSTALLED ACCORDINGLY WHEN IN PAKED AND TRAFFIC AREAS. ALL STOM SEVER MANHOLES IN PAKED AREAS SHALL BE FUSH WITH THE PAKENET AND SHALL HAVE TRAFFIC BEARING RINGS AND COVERS. MANHOLES IN UNPAKED AREAS SHALL BE 1" ABOVE FINISH GRADING LIDS SHALL BE LABELED PER JURSBOTCHORAL SPECIFICATIONS. STE GRADING SHALL NOT PROCEED UNTIL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. THE CONTRACTOR SHALL ADHERE TO ALL TERMS AND CONDITIONS AS OUTLINED IN THE GENERAL TANDEDS EPRIMIT AND THE SWEPP FOR STORMMATED DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES. I. ALL UNSURNACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL TO FINAL GRADE. RECEIVE TO THE LANDSCAPE PLAN. I. ALL UNSURNED INFORMATED DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES. I. TOPOGRAPHIC MEDINATION TAKEN FROM A TOPOGRAPHIC SURVEY BY LAND SURVEYORS. IF CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS THROUGHONT ALL PHASES OF CONSTITUTION. I. CONTRACTOR SHALL RESET TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTBULES. SLOPE PANKING, SUPWALKS, EXIT PRORHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT UTILITY ENTRANCE LOCATIONS. I.S THE CARTINGOR FUNDATIONS AND SLABS SHALL BE IN ACCORDANCE WITH ARCHITECTURAL BUILDING FOINDATIONS AND SLABS SHALL BE IN ACCORDANCE WITH TH AND CONTINUOUS GRADE. I. CONTRACTOR SHALL AUSUST AND/OR OUT EXISTING PARKING, SDEWALKS, DRIVES, ETC. CLEAR AND FREE FROM ANY CONSTRUCTION ACTIVITY AND/OR AUTERIAL TO ENSURE LASY AND SAFE PEDESTRIAN AND WEHOLUAR TRAFFIC TO AND FROM THE SITE. I. CONTRACTOR SHALL AUJUST AND/OR CUT EXISTING PARKING, SDEWALKS, DRIVES, ETC. CLEAR AND FREE FROM ANY CONSTRUCTION ACTIVITY AND/OR AUTERIALITO. ENSURE EASY AND SAFE PEDESTRIAN AND WEHOLUAR TRAFFIC TO AND FROM THE SITE. I. CONTRACTOR SHALL AUJUST AND/OR AUTERIALITIC ENSURES AND STEME. I. CONTRACTOR SHALL MUNITAIN ALL EXISTING PARKMENT AS INCESSARY TO ENSURE A SMOOTH IT AND CONTINUOUS GRADE. I. FINE TRACES ARE ENCORDING MARE	WALLS, JERS. ANY THE WALL G SEPARATE REGISTERED ASSOCIATED SAFETY BASED ON KEN IN THE BEFORE ANY CHT-OF-WAY. OTED. BE THE NECESSARY DNNECTION UNIFORM WALLS, GATEWAY PLAZA W. HUDSON RD, ROGERS, AR BEFORE ANY CHT-OF-WAY. COTED. DISCUSSION COTED. C	 THE DESIGN, INSPECTION, AND CERTIFICATION OF ANY RETAINING WALL SHOWN OR REFERENCED HEREIN, INCLUDING BUT NOT LIMITED TO, SEGMENTAL RETAINING WALLS, MASS GRAVITY WALLS, GABION WALLS, ETC., GREATER THAN FORTY-EIGHT INCHES IN HEIGHT, SHALL BE BY OTHERS. ANY RETAINING WALL DATA SHOWN OR REFERENCED HEREIN SHALL BE FOR COORDINATION OF THE WALL LOCATION AND ELEVATIONS ONLY. THE OWNER/CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR OBTAINING AND PROVIDING SEPARATE AND INDEPENDENT RETAINING WALL DESIGNS, INSPECTIONS, AND CERTIFICATIONS BY A REGISTERED PROFESSIONAL ENGINEER OTHER THAN CRAFTON TULL. THIS SHALL ALSO APPLY TO ANY ASSOCIATED AND NECESSARY PUBLIC SAFETY DEVICES INCLUDING, BUT NOT LIMITED TO, PEDESTRIAN SAFETY RAILS. THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF EXISTING UTILITIES ON SITE OR IN RIGHT-OF-WAY. ALL UTILITIES MUST BE LOCATED PRIOR TO GRADING START. ALL WORK SHALL BE DONE IN STRICT ACCORDANCE WITH THE PROJECT SPECIFICATIONS. ALL CUT OR FILL SLOPES SHALL BE A MAX 3:1 SLOPE OR FLATTER UNLESS OTHERWISE NOTED. IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITION OR BETTER. ALL STORM SEWER PIPE CONNECTIONS TO STRUCTURES SHALL BE GROUTED TO ENSURE CONNECTION AT STRUCTURE IS WATERTIGHT. ALL STORM SEWER STRUCTURES SHALL HAVE A SMOOTH UNIFORM POURED MORTAR INVERT FROM INVERT IN TO INVERT OUT. 	1. 2. 3. 4. 5. 6. 7.
CRAFTON TULL CANNOT CERTIFY THE SITE AS COMPLETE IN ORDER	D SHALL ABOVE ABOVE MEASURES S AS ASSOCIATED TOPSOIL TO IF INGINEER TURAL AND NONS OF DIMENSIONS WITH ING PIPES A SMOOTH D FREE TRIAN AND E TINISH FLOOR IM BUILDING IE PROPER CALLY, THE WE THE RCEL OR AS This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Crafton, Tull & Associates, Inc., and is not to be used, in whole or in part, for any other project, without the written outhorization of Crafton, Tull & Associates, Inc., MATURE AND IS NOT HIS DER PRELIMINARY PLANS IP CALLY, THE WE THE RELIMINARY IN PLANS PRELIMINARY IN PLANS IP CALLY, THE WE THE RELIMINARY IN PLANS	LOADING AND BE INSTALLED ACCORDINGLY WHEN IN PAVED AND TRAFFIC AREAS. ALL STORN SEVER MANHOLES IN PAVED AREAS SHALL BE FLUSH WITH THE PAVEMENT AND SHALL HAVE TRAFFIC BEARING RINGS AND COVERS. MANHOLES IN UNPAVED AREAS SHALL BE 1° ABOVE FINISH GRADE. LIDS SHALL BE LABELED PER JURISDICTIONAL SPECIFICATIONS. IS TE GRADING SHALL NOT PROCEED LINITL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. THE CONTRACTOR SHALL ADHERE TO ALL TERMS AND CONDITIONS AS OUTLINED IN THE GENERAL INPDES PERMIT AND THE SWPPP FOR STORMWATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITES. 11. ALL UNSUFFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL TO FINAL GRADE. REFER TO THE LANDSCAPE PLAN. 12. TOPOGRAPHIC INFORMATION TAKEN FROM A TOPOGRAPHIC SURVEY BY LAND SURVEYORS. IF CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON PLANS, CONTACT ENGINEER IMMEDIATELY. 13. THE CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS THROUGHOUT ALL PHASES OF CONSTRUCTION. 14. CONTRACTOR SHALL REFET TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT UTILITY ENTRANCE LOCATIONS. 15. THE EARTHWORK FOR ALL BUILDING FOUNDATIONS AND SLABS SHALL BE IN ACCORDANCE WITH ARCHITECTURAL BUILDING PLANS AND SPECIFICATIONS. 16. CONTRACTOR SHALL ASCUTURES TO BE INSPECTED AND REPAIRED AS NEEDED, AND EXISTING PIPES 17. DE CLEANED OUT TO REMOVE ALL SILT AND DEBRIS. 17. CONTRACTOR SHALL MUST AND/OR CUT EXISTING PARKING, SIDEWALKS, DRIVES, ETC. CLEAR AND EXISTING PIPES 18. CONTRACTOR SHALL MUST AND/OR CUT EXISTING PARKING, SIDEWALKS, DRIVES, ETC. CLEAR AND EXISTING PIPES 19. DE CLEANED OUT TO REMOVE ALL SILT AND DEBRIS. 10. CONTRACTOR SHALL MUST AND/OR THE ENCONTRACTOR SHALL COORDINATE WITH THE GEOTECHNICAL ENGINEER FOR THE DESIGN AND PLACEMENT AS NECESSARY TO ENSURE A SMOOTH FIT AND CONTINUOUS GRADE. 10. CONTRACTOR SHALL MENTAND ALL EXI	 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19.



EROSION CONTROL LEGEND		
PS PS	PERMANENT SEEDING	
	TEMPORARY PARKING AND STORAGE	
	BOUNDARY LINE RIGHT OF WAY LINE LIMITS OF DISTURBANCE GRADE BREAK CONTOUR ELEVATIONS STORM DRAIN	
XX.X%	DIRECTION OF OVERLAND FLOW W/ GRADE	
\leftarrow	LIMITS OF DRAINAGE SUB-BASIN	
CD	ROCK CHECK DAM	
CE	STABILIZED CONSTRUCTION EXIT (ENTRANCE)	
DD	CHANNELED DIVERSIONS	
DS	DEWATERING SYSTEM / STRUCTURE	
SF SF	SILT FENCE	
	SEDIMENT BASIN WITH STONE OUTLET	
🛞 IP1	BLOCK AND AGGREGATE INLET SEDIMENT DEVICE	
ØIP2	CURB INLET FILTER SOCK	
ØIP3	GRATED INLET GRAVEL SEDIMENT FILTER	
IP4	SILT FENCE INLET PROTECTION	
OP1	RIP_RAP_SLOPE_PROTECTION (SEE_SIZE_THIS_SHEET)	
ECL	PERMANENT EROSION CONTROL LINING	
🖛 SB 🖚 SB	TEMPORARY SEDIMENT BASIN	
FFT FFT	FIBER FLOCCULENT TUBE	

NOTE: SEE SITE PLAN FOR EXISTING LEGEND SYMBOLS

SITE STABILIZATION AND CERTIFICATION

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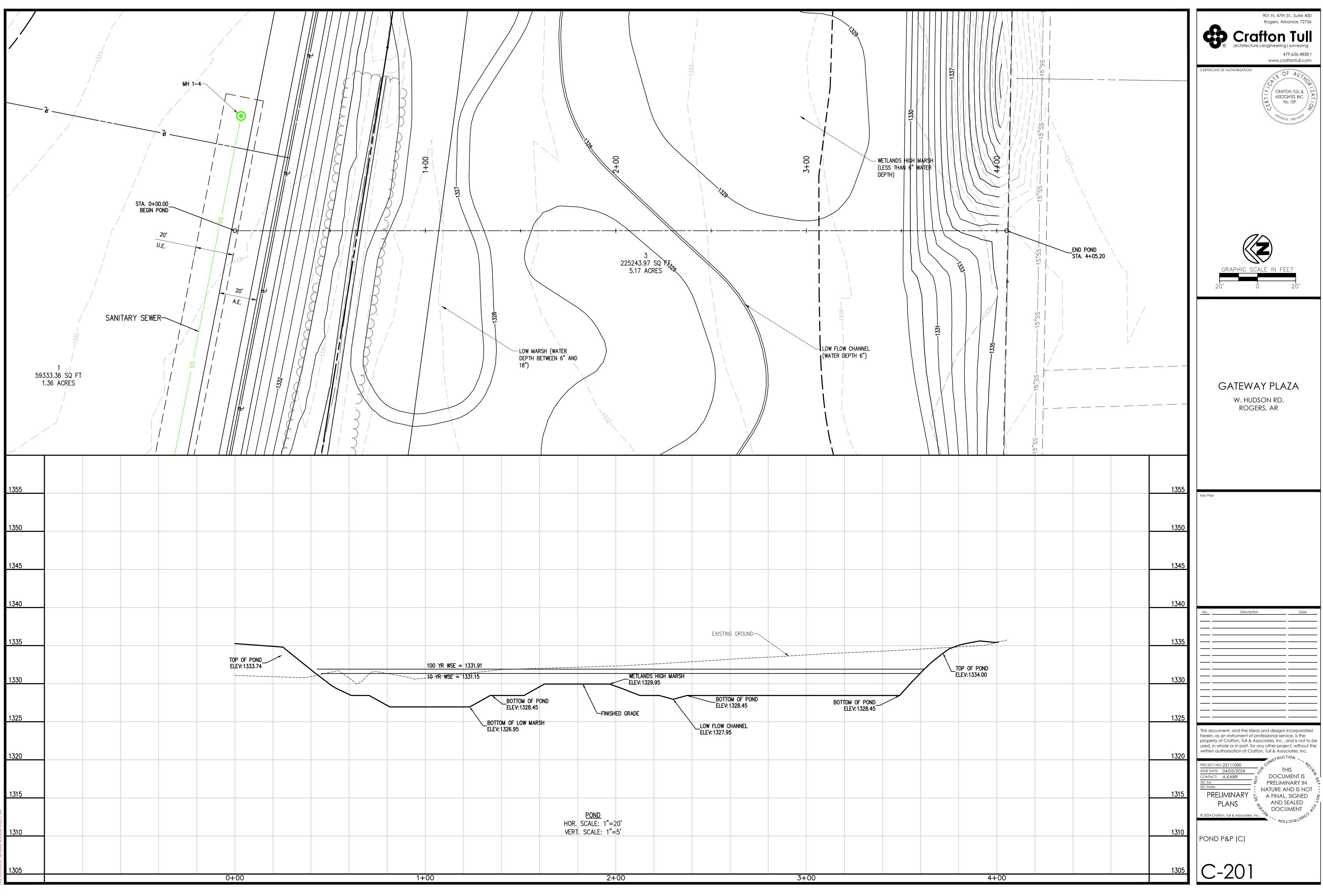
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FFT FFT FFT	FIBER FLOCCULENT TUBE		

NOTE: SEE SITE PLAN FOR EXISTING LEGEND SYMBOLS

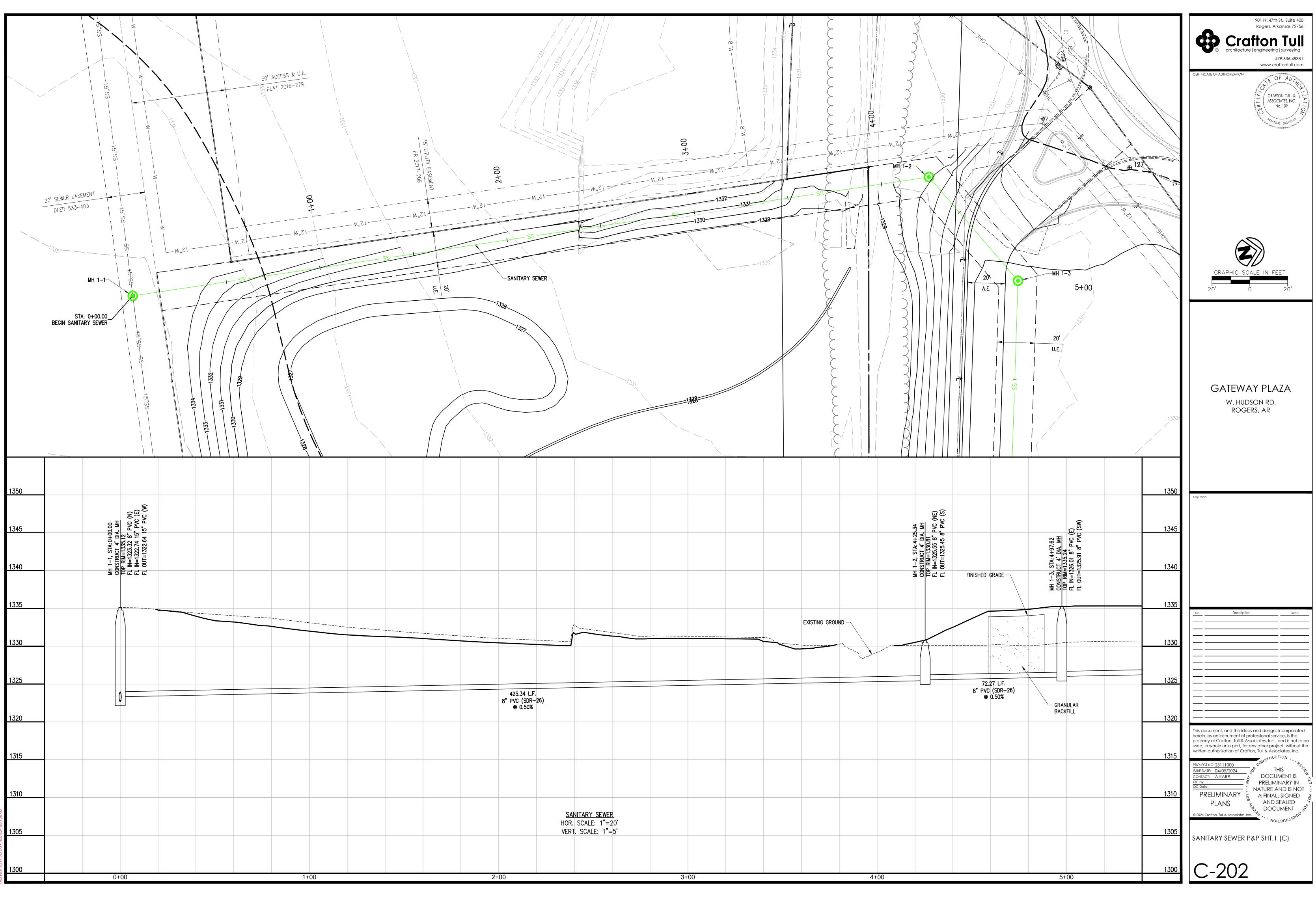
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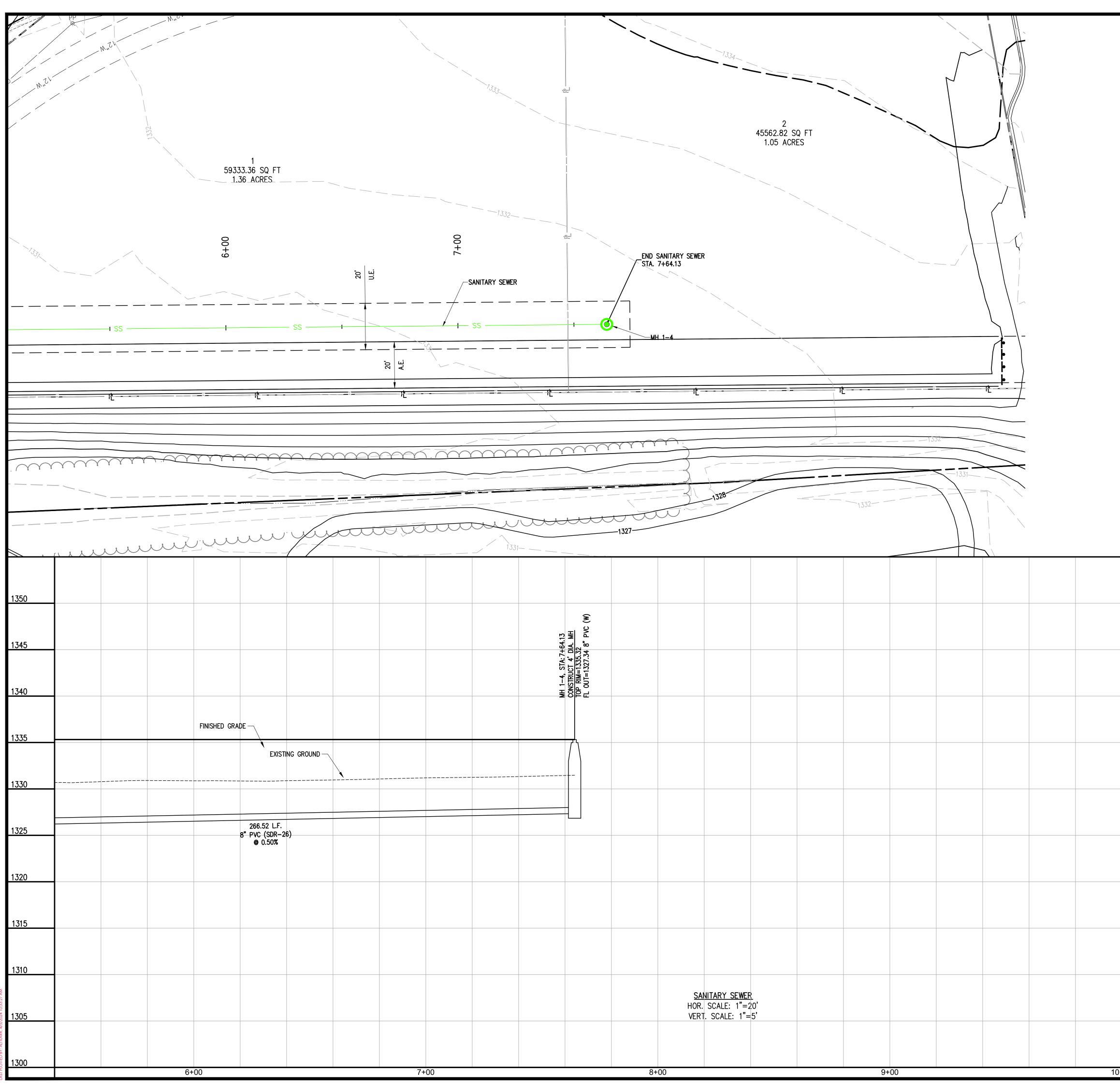
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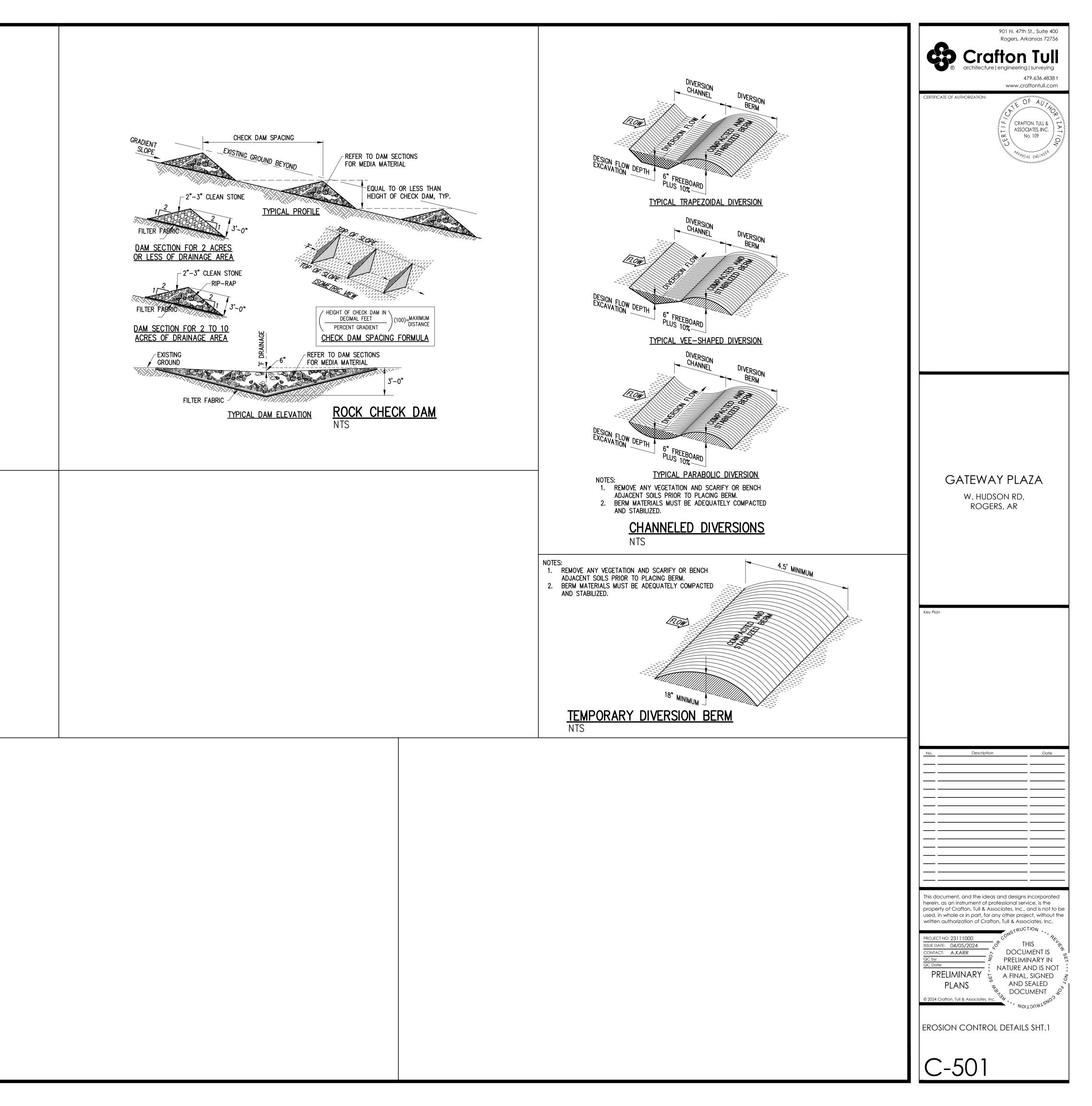
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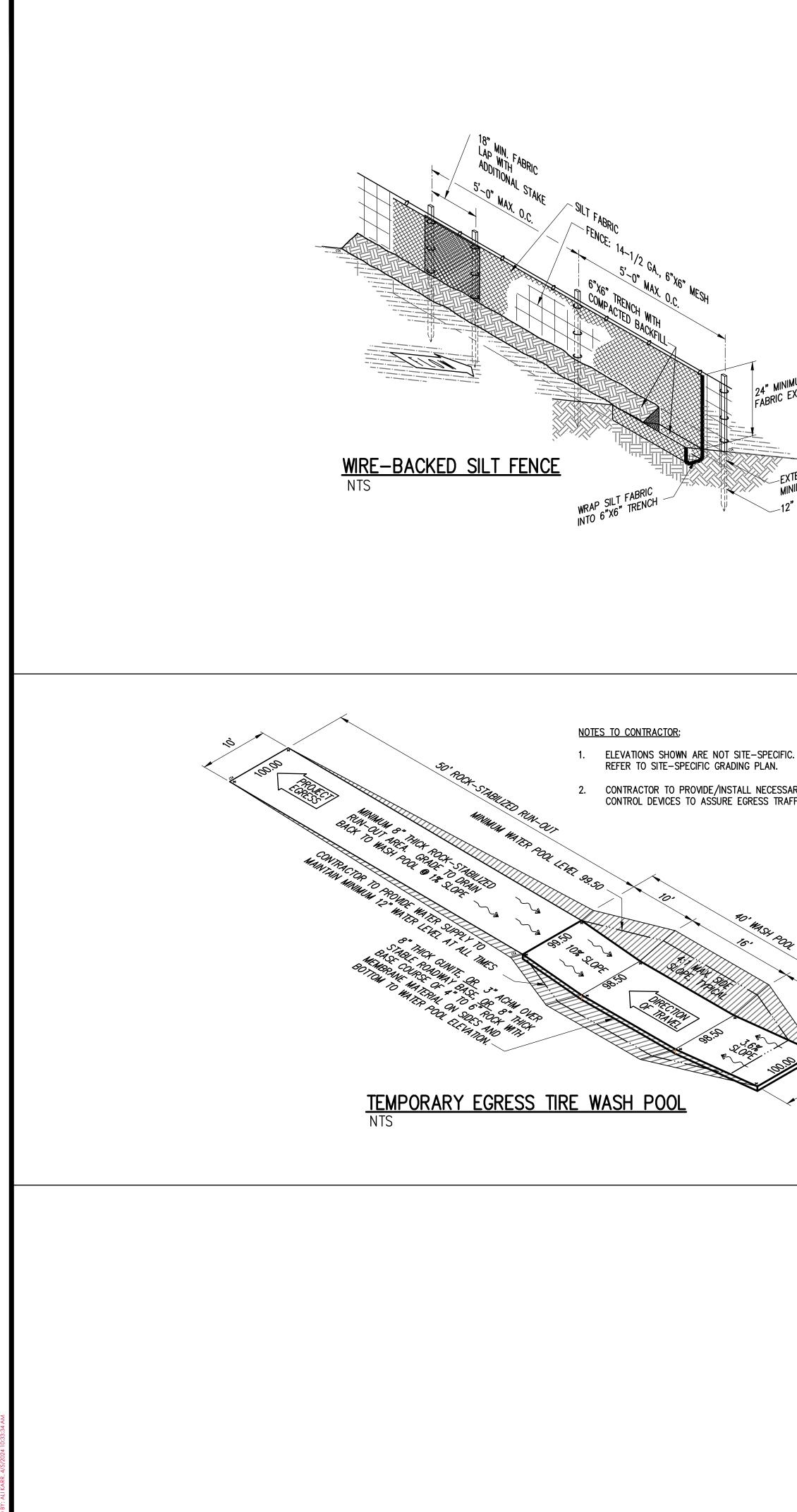


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	 PHASE I INSTALL STABILIZED CONSTRUCTION ENTRANCES/EXITS. PREPARE TEMPORARY PARKING AND STORAGE AREAS. UPON IMPLEMENTATION AND INSTALLATION OF THE FOLLOWING: TRAILER, PARKING, LAY DOWN, PORTA-POTTY, WHEEL WASH, CONCRETE WASH-OUT, MASON'S AREA, FUEL AND MATERIAL STORAGE CONTAINERS, SOLID WASTE CONTAINERS, ETC., DENOTE THEM ON THE SITE MAPS IMMEDIATELY AND NOTE ANY CHANGES IN THE LOCATIONS AS THEY OCCUR THROUGHOUT THE CONSTRUCTION PROCESS. CONSTRUCT THE SILT FENCES ON THE SITE. CONSTRUCT THE SEDIMENTATION AND SEDIMENT TRAP BASINS. HALT ALL ACTIVITIES AND CONTACT THE CIVIL ENGINEER CONSULTANT TO PERFORM INSPECTION OF BMPs. GENERAL CONTRACTOR SHALL SCHEDULE AND CONDUCT STORM WATER PRE-CONSTRUCTION MEETING WITH ENGINEER AND ALL GROUND-DISTURBING CONTRACTORS BEFORE PROCEEDING WITH CONSTRUCTION. CLEAR AND GRUB THE SITE. START CONSTRUCTION OF THE BUILDING PAD AND STRUCTURES. BEGIN GRADING THE SITE. 	PROVIDE CONTINUOUS RAISED PERIMETER STRUCTURE/BARRIE (SUCH AS STRAW BALES OR 2 AND PROVIDE ADEQUATE MEAN SECURE TO GROUND.
4" MINIMUM ABRIC EXPOSURE	 PHASE II 1. TEMPORARILY SEED DENUDED AREAS. 2. INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS. 3. INSTALL RIP-RAP AROUND OUT STRUCTURES. 4. INSTALL INLET PROTECTION AROUND ALL STORM SEWER STRUCTURES. 5. PREPARE SITE FOR PAVING. 6. PAVE SITE. 7. INSTALL INLET PROTECTION DEVICES. 8. COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING. 9. REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IF SITE IS STABILIZE 	CC NTS
EXTEND WRE FENCE 2" MINIMUM INTO TRENCH 12" MINIMUM STAKE BURY	 <u>GENERAL EROSION NOTES:</u> A. THE STORMWATER POLLUTION PREVENTION PLAN IS COMPRISED OF THIS DRAWING (SITE MAP), THE STANDARD DETAILS, THE PLAN NARRATIVE, ATTACHMENTS INCLUDED IN THE SPECIFICATIONS SECTION 312800 (EROSION AND SEDIMENTATION CONTROL), PLUS THE PERMIT AND ALL SUBSEQUENT REPORTS AND RELATED DOCUMENTS. B. ALL CONTRACTORS AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE STORM WATER POLLUTION PREVENTION PLAN 	NOTE: GENERAL CONT CONSTRUCTION SEQUENCE ROUGH GRADE / SEDIMENT CONTROL TEMPORARY CONTROL MEASURES STRIP & STOCKPILE TOPSOIL
	(SWPPP) AND THE STATE OF ARKANSAS NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM GENERAL PERMIT (NPDES PERMIT) AND BECOME FAMILIAR WITH THEIR CONTENTS. C. CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES AS REQUIRED BY THE SWPPP. ADDITIONAL BEST MANAGEMENT PRACTICES SHALL BE IMPLEMENTED AS DICTATED BY CONDITIONS AT NO ADDITIONAL COST OF OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION.	STORM FACILITIES TEMPORARY CONSTRUCTION ROADS FOUNDATION / BUILDING CONSTRUCTION
	 D. BEST MANAGEMENT PRACTICES (BMP) AND CONTROLS SHALL CONFORM TO FEDERAL, STATE OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE, AND APPLICABLE CONTRACTOR SHALL IMPLEMENT ADDITIONAL CONTROLS AS DIRECTED BY THE PERMITTING AGENCY OR OWNER. E. SITE MAP MUST CLEARLY DELINEATE ALL STATE WATERS. PERMITS FOR ANY CONSTRUCTION ACTIVITY IMPACTING STATE WATERS. OR DECLINATED WETLANDS MUST BE 	SITE CONSTRUCTION PERMANENT CONTROL STRUCTURES FINISH GRADING LANDSCAPING/SEED/FINAL STABILIZATION
SPECIFIC. LAN. IECESSARY TRAFFIC SS TRAFFIC USE.	CONSTRUCTION ACTIVITY IMPACTING STATE WATERS OR REGULATED WETLANDS MUST BE MAINTAINED ON THE SITE AT ALL TIMES. F. CONTRACTOR SHALL MINIMIZE CLEARING TO THE MAXIMUM EXTENT PRACTICAL OR AS REQUIRED BY THE GENERAL PERMIT. G. GENERAL CONTRACTOR SHALL DENOTE THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, AND AREA FOR PORTABLE FACILITIES, OFFICE TRAILERS AND TOILET FACILITIES.	
	 H. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE AND EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED. I. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN UP FUEL OR CHEMICAL SPILLS AND LEAKS. J. DUST ON THE SITE SHALL BE CONTROLLED. THE USE OF MOTOR OILS AND OTHER 	ACREAGE IMPERVIOUS AREA SEEDED AREA
134 POQ 14-	PETROLEUM-BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED. K. RUBBISH, TRASH, GARBAGE, LITTER OR OTHER SUCH MATERIALS SHALL BE DEPOSITED INTO SEALED CONTAINERS. MATERIALS SHALL BE PREVENTED FROM BEING BLOWN OR WASHED OFF-SITE. L. ALL STORM WATER POLLUTION PREVENTION MEASURES PRESENTED ON THIS PLAN AND SWPPP SHALL BE INITIATED AS SOON AS POSSIBLE.	TOTAL DISTURBED
	 M. DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS STOPPED FOR AT LEAST 14 DAYS SHALL BE TEMPORARY SEEDED. N. DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS PERMANENTLY STOPPED SHALL BE PERMANENTLY SEEDED NO LATER THAN 14 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY OCCURRING IN THESE AREAS. REFER TO THE GRADING PLAN AND/OR LANDSCAPE PLAN. 	ENTER BENCHMARK INFORMATION
~	 O. IF THE ACTION OF VEHICLES TRAVELING OVER THE GRAVEL CONSTRUCTION ENTRANCE/EXIT IS NOT SUFFICIENT TO REMOVE THE MAJORITY OF DIRT OR MUD, THEN THE TIRES MUST BE WASHED BEFORE THE VEHICLES EXIT ONTO THE PUBLIC ROADS. IF WASHING IS USED, PROVISIONS MUST BE MADE TO INTERCEPT THE WASH WATER AND TRAP THE SEDIMENT BEFORE IT IS CARRIED OFF-SITE. P. ALL MATERIALS SPILLED, DROPPED, WASHED OR TRACKED FROM VEHICLES ONTO ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY. 	Lost Springs Golf & Athletic Club ROLLING
	 Q. CONTRACTORS OR SUBCONTRACTORS WILL BE RESPONSIBLE FOR REMOVING SEDIMENT IN THE DETENTION POND AND ANY SEDIMENT THAT MAY HAVE COLLECTED IN THE STORM SEWER DRAINAGE SYSTEMS IN CONJUNCTION WITH THE STABILIZATION OF THE SITE. R. ON-SITE AND OFF-SITE STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BEST MANAGEMENT 	Idson Rd W Hudson Rd VAN DYKE CENTER
	 S. SLOPES SHALL BE LEFT IN A ROUGHENED CONDITION DURING THE GRADING PHASE TO REDUCE RUNOFF VELOCITIES AND EROSION. T. DUE TO THE GRADE CHANGES DURING THE DEVELOPMENT OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION CONTROL MEASURES (SILT FENCES, STRAW BALES, ETC.) TO PREVENT EROSION. 	RTHRIDGE ESTRIDGE BISHOP MA
	U. ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY. THIS INCLUDES BACKFILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.	VILLAGE

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